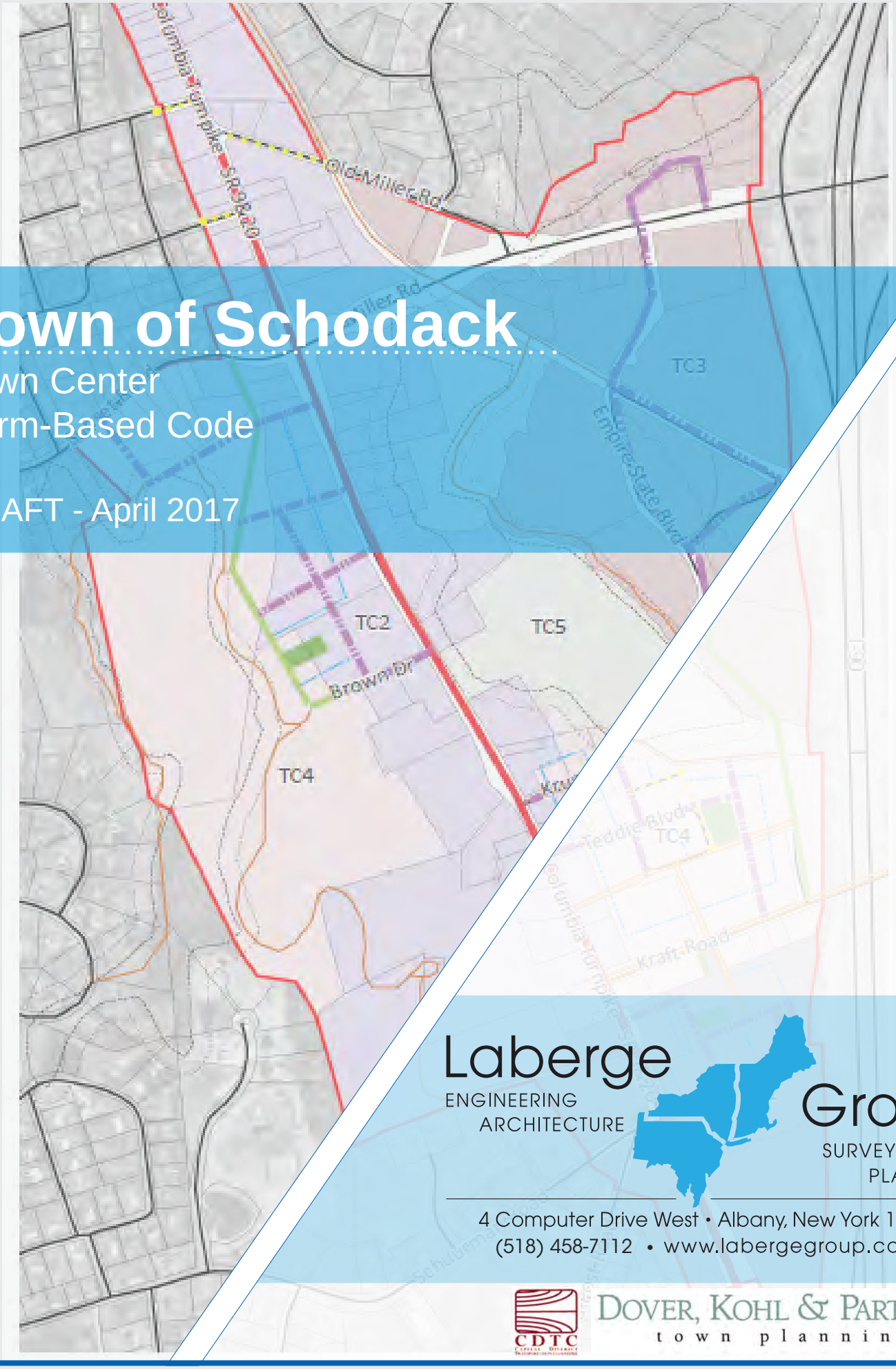



# Town of Schodack

## Town Center
## Form-Based Code

DRAFT - April 2017

Laberge Group
ENGINEERING
ARCHITECTURE
SURVEYING
PLANNING

4 Computer Drive West • Albany, New York 12205
(518) 458-7112 • www.labergegroup.com

CDTC
DOVER, KOHL & PARTNERS
town planning

“This report was funded in part through grant[s] from the Federal Highway Administration [and Federal Transit Administration], U.S. Department of Transportation. The views and opinions of the authors expressed herein do not necessarily state or reflect those of the U. S. Department of Transportation. The recommendations in this study are conceptual in nature and do not commit NYSDOT [or other entities] to a capital transportation project(s)."

## §219-_Introduction ..... 2

## §219-_Reserved ..... 3

## §219-_Town Center Districts ..... 4

3-1 Purpose and Intent ..... 4
3-2 Regulating Plan Map ..... 5
3-3 Permitted Uses Table ..... 6
3-4 (TC1) Town Center Core ..... 8
3-5 (TC2) Town Center Corridor ..... 12
3-6 (TC3) Town Center General ..... 16
3-7 (TC4) Town Center Edge ..... 20
3-8 (TC5) Town Center Preserve ..... 24

## §219-_General Standards ..... 26

4-1 Purpose and Intent ..... 26
4-2 Buildings Standards ..... 26
4-3 Facades ..... 26
4-4 Site Standards ..... 34
4-5 Parking Standards ..... 34
4-6 Signage Standards ..... 34
4-7 Lighting Standards ..... 35

## §219-_Future Streets & Civic Spaces ..... 36

5-1 Future Streets ..... 36
5-2 Civic Spaces ..... 37
5-3 Future Streets and Civic Spaces Map ..... 38
5-4 Future Street Types ..... 39

## Definitions ..... 45

## Appendices

Appendix A: Public Outreach

Appendix B: Environmental Justice

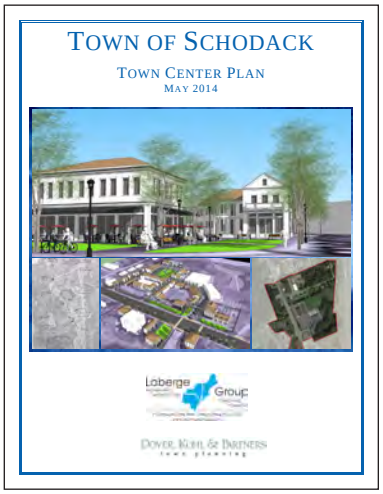
# §219-_ Introduction

## THE SCHODACK TOWN CENTER PLAN

The Town of Schodack seeks to develop a walkable "town center" along SR 9&20 between exits 10 and 11 on I-90 and across from Town Hall.

Town officials and the Schodack community, with the assistance of planning consultants Laberge Group and Dover, Kohl & Partners, worked to imagine how this area of town could evolve over time with redevelopment into an attractive mixed-use town center. The goal is to create a rich pedestrian atmosphere and active urban identity for the heart of town.

A public workshop was held in November 2012 where local residents, business owners, and property owners were invited to participate and discuss their vision for the look, feel, and function of a Town Center. The result of these dialogs were used to shape the Town of Schodack Town Center Plan, which was accepted by the Town Council in 2014. Funding for this effort was provided by the Capital District Transportation Committee (CDTC).




## HOW AND WHY THIS CODE WAS CREATED

It was understood that in order to realize the vision outlined in the Town Center Plan, the existing zoning for this area of town would need to be revised.

Form-based zoning places special emphasis on the physical configuration of the built environment and utilizes graphics to explain what the desired form and appearance of development should be—with an eye toward creating beautiful, highly livable places.

With the assistance of additional funding by the CDTC, follow-up work was commenced in 2016 to develop a new form-based zoning code which would allow and encourage the predictable type of redevelopment outlined in the Town Center Plan.

An additional workshop was held in May 2016 to discuss the specific details and regulating portions of this form-based zoning. The input received from the public, town officials, landowners, and business owners was used to shape this code.

A small pedestrian square on the NE corner of Schuurman Rd and SR 9 & 20.

The heart of the Schodack Town Center facing SR 9 & 20.

# §219-_ Town Center Districts

## 3-1 Purpose and Intent

The Schodack Town Center Code hereby establishes the following districts, as shown on the Regulating Plan Map herein.

Each Town Center District is defined by particular characteristics that correspond with building placement, building form, and frontage standards, all of which influence the level of walkability and vibrancy in a particular place.

The Town Center Districts for the Schodack include:

**TC1: TOWN CENTER CORE**
This district forms the core of Schodack's Town Center. Priority is placed here on optimizing the physical characteristics of the built environment for increased walkability. While much of the land encompassed by this district was originally built in an automobile-dominant format, the intent is to facilitate a transition of individual parcels over time, each adding up to a vibrant, walkable town center.

To maximize vibrancy and walkability, this district features buildings close to the sidewalk, plentiful shade for pedestrians, and parking lots screened from view.

**TC2: TOWN CENTER CORRIDOR**
This district is located where Schodack's Town Center extends north along the Columbia Turnpike. Buildings are required to be street-oriented, but their placement is somewhat more flexible than in other Town Center districts, to allow adjustment to accommodate features such as wider outdoor terraces and green spaces.

**TC3: TOWN CENTER GENERAL**
This district governs development near the intersection of Interstate 90 and Miller Road. While oriented primarily toward the highway, the built environment in this area is intended to retain key features of walkability.

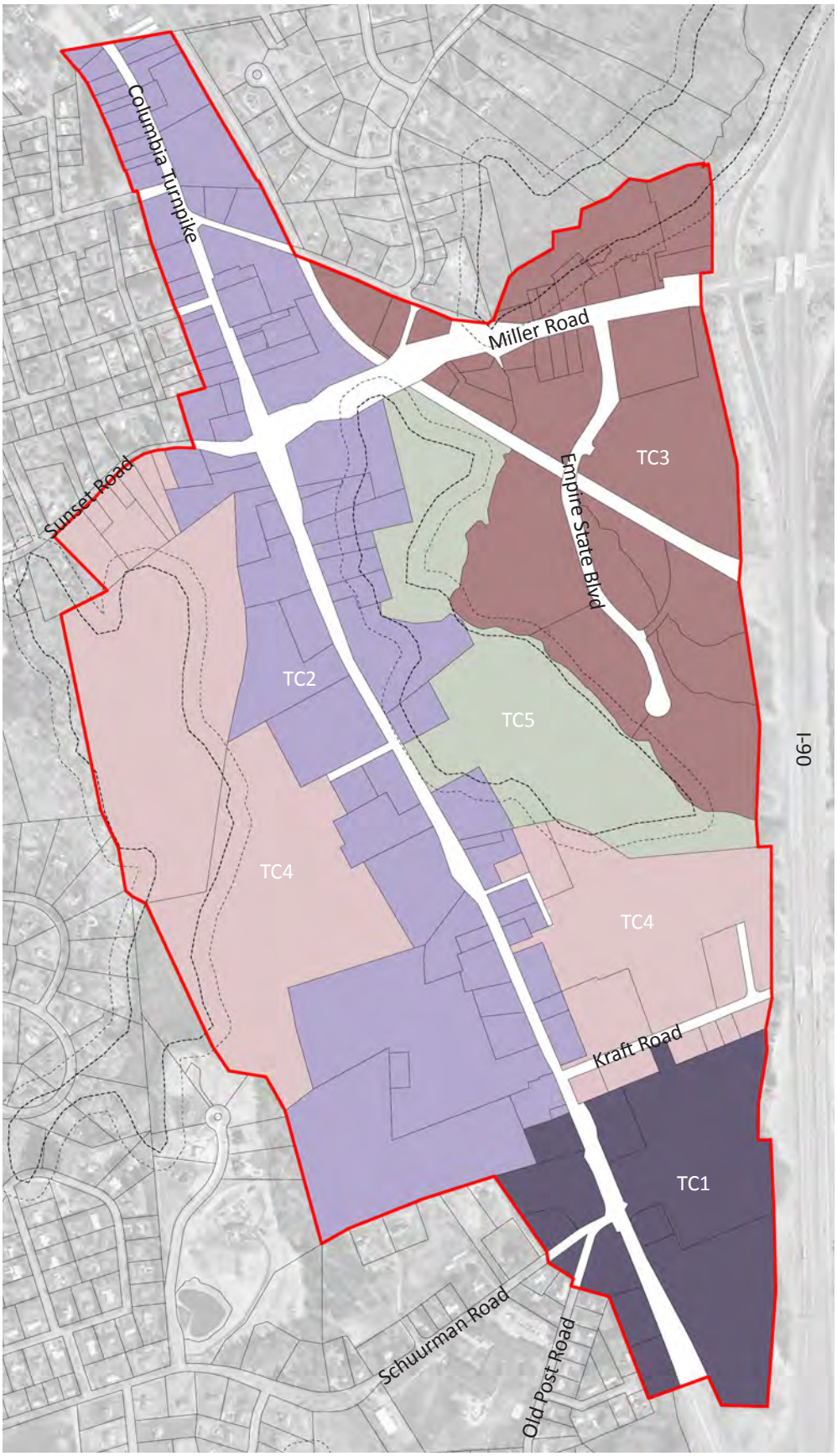
**TC4: TOWN CENTER EDGE**
The intent of this district is to facilitate a transition between the vibrant and urbane TC5 Town Center Core and lower intensity development and Preserve areas on the periphery of Schodack's Town Center.

Among other adjusted details, buildings in this mixed-use district are predominantly residential, are set slightly further back from the street and heights and are somewhat shorter than those in the TC5 District.

**TC5: PRESERVE**
The Preserve district consists of property that is primarily wetland. Development within this area is limited primarily due to wetland requirements, wetland buffers, and steep slopes. Limited boardwalk trails may be developed in order to access the area for recreational purposes.

## 3-2 Regulating Plan Map

- TC1: Town Center Core
- TC2: Town Center Corridor
- TC3: Town Center General
- TC4: Town Center Edge
- TC5: Preserve
- Town Center Form-Based Code Boundary
- Parcels
- Wetland
- Wetland Buffer



## 3-3 Permitted Uses Table

The allowable uses in each Town Center District are as set forth in the Permitted Uses Table herein.

| RESIDENTIAL | TC1 | TC2 | TC3 | TC4 |
|---|---|---|---|---|
| Accessory Apartment | A | A | A | A |
| Dwelling, Multifamily | P | P | P | P |
| Dwelling, One-Family | A | P | | P |
| Dwelling, Senior Congregate | S | | | S |
| Dwelling, Townhouse | P | P | P | P |
| Dwelling, Two-Family | P | P | | P |
| Farm Labor Housing | | | | |
| Mixed Use Development Residential | P | | | |

| LODGING SERVICES | | | | |
|---|---|---|---|---|
| Bed-and-Breakfast | P | P | P | P |
| Hotel & Motel | P | P | P | |
| Renting of Rooms (2 Max) | A | A | A | A |

| OFFICE | | | | |
|---|---|---|---|---|
| Medical Facility | P | P | P | S |
| Office, Business and Professional | P | P | P | |
| Office, Corporate Center & Park | | | P | |
| Retail and Service Establishment | P | P | P | |
| Veterinarian Faciility | S | P | S | |

| BUSINESS SERVICES | | | | |
|---|---|---|---|---|
| Antique Shop | P | S | S | S |
| Building Supply Facility | S | | | |
| Dry Cleaning & Laundry Establishment | S | | S | |
| Drive Thru | | S | S | |
| Farm Market | S | | | |
| Food Truck | A | A | A | |
| Mixed Use Developments | P | P | P | P |
| Restaurant | P | P | P | P |
| Retail and/or Service Establishment | P | P | P | A |
| Roadside Stand | A | A | A | A |
| Shopping Center | S | P | | |
| Theater | P | | | |

P = Permitted Principal Uses
A = Permitted Accessory Uses
S = Special Permit Uses

| CIVIC | TC1 | TC2 | TC3 | TC4 |
|---|---|---|---|---|
| Cemetery | | | | |
| Civic Center | | | | |
| Funeral Home | | | | |
| Government Building & Facilities | P | P | P | S |
| Club, Membership-Club | P | P | P | A |
| Public Utilities | S | S | S | S |
| Recreation Facility | | | | |
| Religious Use or Assembly | P | P | P | A |

| EDUCATION | | | | |
|---|---|---|---|---|
| Day Care Center, Child and Adult | S | S | S | S |
| Schools, Public & Private | | | | |

| AUTOMOTIVE / TRANSPORTATION | | | | |
|---|---|---|---|---|
| Boat Sales & Repairs | | | | |
| Boat Storage | | | | |
| Car Wash | | P | | |
| Marina | | | | |
| Motor Vehicle Service, Fuel Sales, & Repair | | P | | |
| Motor Vehicle Sales Establishment | | P | | |
| Shipbuilding | | | | |
| Storage of Construction Vehicles | | | S | |
| Terminal | | | | |

| OTHER: GENERAL | | | | |
|---|---|---|---|---|
| Camp, Day | | | | |
| Clothing Bins (Accessory to Religious, not for Profits) | A | A | A | A |
| Accessory Uses (Customary) | A | A | A | A |
| Kennel | | | | |
| Telecommunications Services | S | S | S | S |
| Recreation Vehicle Parks | | | | |

| OTHER: AGRICULTURAL / INDUSTRIAL | TC1 | TC2 | TC3 | TC4 |
|---|---|---|---|---|
| Agricultural Activitys | | | | |
| Agricultural Uses, Animals | | | | |
| Agricultural Uses, Crops | | | | |
| Agribusiness | | | | |
| Agritourism | | | | |
| Cogeneration Plants | | | | |
| Construction & Landscaping Services | | | | |
| Manufacturing, Heavy | | | | |
| Manufacturing, Light | | | P | |
| Research & Development Facilities | | | | |
| Timber Harvesting and Forestry | | | | |
| Terminal | | | | |
| Warehouse | | | S | |

TC1

## 3-4 Town Center Core

### Overview

This district forms the core of Schodack's Town Center. Priority is placed here on optimizing the physical characteristics of the built environment for increased walkability. While much of the land encompassed by this district was originally built in an automobile-dominant format, the intent is to facilitate a transition of individual parcels over time, each adding up to a vibrant, walkable town center.

To maximize vibrancy and walkability, this district features buildings located close to the sidewalk, plentiful shade for pedestrians, and parking lots screened from view.

*Illustrative example of buildings and site arrangement in the TC1 Town Center Core District.*

# TC1

## Town Center Core

Examples

*Mixed-use buildings with simple basic massing, well-designed storefronts, and architecturally rich details.*

*A single-story commercial building with a generous ceiling height.*

*A street of commercial block buildings of varying heights and widths.*

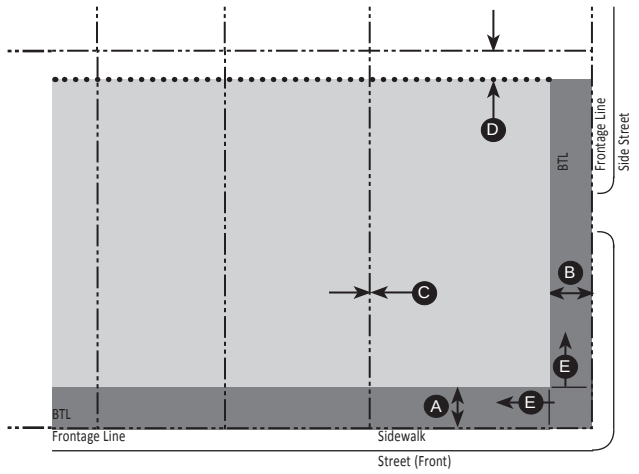
*A shopfront building with pedestrian protection from sun and rain provided by a projecting second floor balcony.*

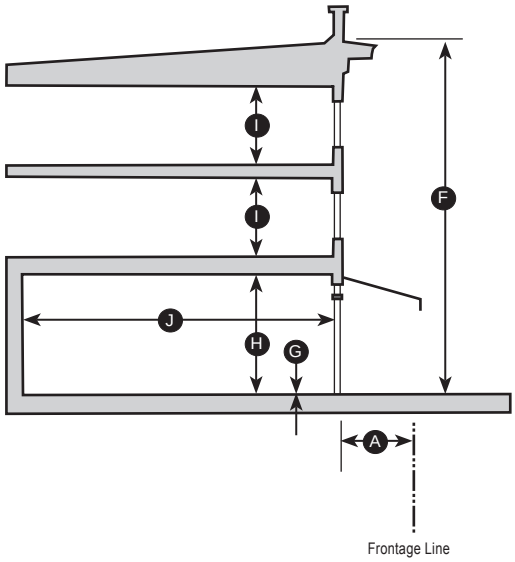
*Large shopfront windows provide interesting views for pedestrians.*

*This civic building, while modest in size, has grand proportions.*

# TC1 Town Center Core

## Form




**Key**

Frontage/Property Line — Setback Line
Build-to Location (BTL) — Building Area

### Building Placement

#### Build-to Locations (Distance from Frontage / Property Line)

| | | |
|---|---|---|
| Front | 0' min., 8'max. | A |
| Side Street | 0' min., 8'max. | B |
| Interior Side Property Line | 0' min. | C |
| Rear | 5' min. | D |

### Frontage Buildout

| | | |
|---|---|---|
| Building Facade at BTL | | |
| Front | 80% min. | E |
| Side Street | 30% min. | |

Street Facades must be built to the BTL for the first 30' on a corner.

### Dedicated Residential Outdoor Open Space

| | |
|---|---|
| Each residential unit shall be provided with unrestricted access to dedicated outdoor open space | 15 square feet min. per unit |
| Distance from Residential Unit | 50' max. |
| Permitted Configurations | Balcony, Porch, Yard, Courtyard, or Roof Terrace. |

### Building Form

#### Height

| | | |
|---|---|---|
| Main Building | 22' min.[1] | F |
| | 4 Stories max.[2] | F |
| Ground Floor Elev. Above Sidewalk | Comm. 6" max., Res. 24" min. | G |
| Ground Floor Office / Retail Ceiling | 14' min. clear | H |
| Ceiling Height | 9' min. clear | I |

[1]Measured from grade to eave or parapet base

[2]See definition of "Story" in Definitions

### Footprint

| | | |
|---|---|---|
| Depth, ground floor commercial space: | 15' min. | J |

### Allowed Frontage Types (See Sec. 3-3 Facades)

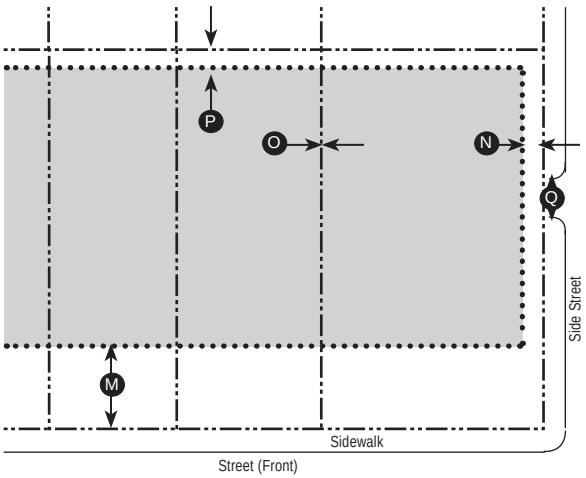
- Shopfront
- Gallery
- Forecourt
- Stoop
- Porch

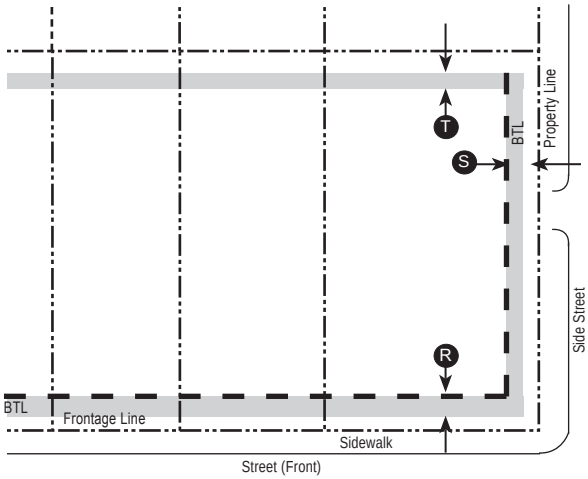
### Allowed Use Types (See Sect. 4-3 Permitted Uses Table)

| | | |
|---|---|---|
| Ground Floor | Office / Retail required within 150' of SR 9&20 Frontage Line | K |
| All Floors Otherwise | All Permitted Uses Allowed | L |

# TC1 Town Center Core

## Parking & Encroachments




**Key**

Frontage/Property Line
Setback Line
Parking Area

**Key**

Frontage/Property Line
Setback Line
Build-to Location (BTL)
Encroachment Area

| Parking (See Sect. 3-5 Parking Standards) | | |
|---|---|---|
| **Parking Location** (Distance from Property Line) | | |
| Front Setback | 30' min. | M |
| Side Street Setback | 30' min. | N |
| Side Setback | 0' min. | O |
| Rear Setback | 5' min. | P |

| Other Allowed Encroachments | | |
|---|---|---|
| Balconies, Bay Windows, Awnings, etc. | | |
| Front | 12' max. | R |
| Side Street | 8' max. | S |
| Rear | 4' max. | T |

| District Specific Parking Requirements | | |
|---|---|---|
| Parking Curb Cut Width | 15' max. | Q |
| No parking spaces are required for buildings that are < 500 sf | | |
| Bicycle parking must be provided and in a secure environment, except in residential buildings with 4 units or less. | | |

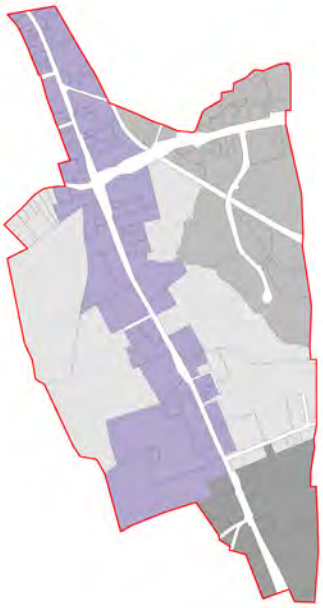
| Miscellaneous |
|---|
| Where a building facade steps back or is absent from the BTL, the BTL line should be maintained and defined by fence, landscape wall or hedge 30"-54" high. |
| All buildings must have a primary entrance along the front facade. |
| Loading docks, overhead doors and other service entries shall not be located on street-facing facades and shall instead be located in rear service areas |

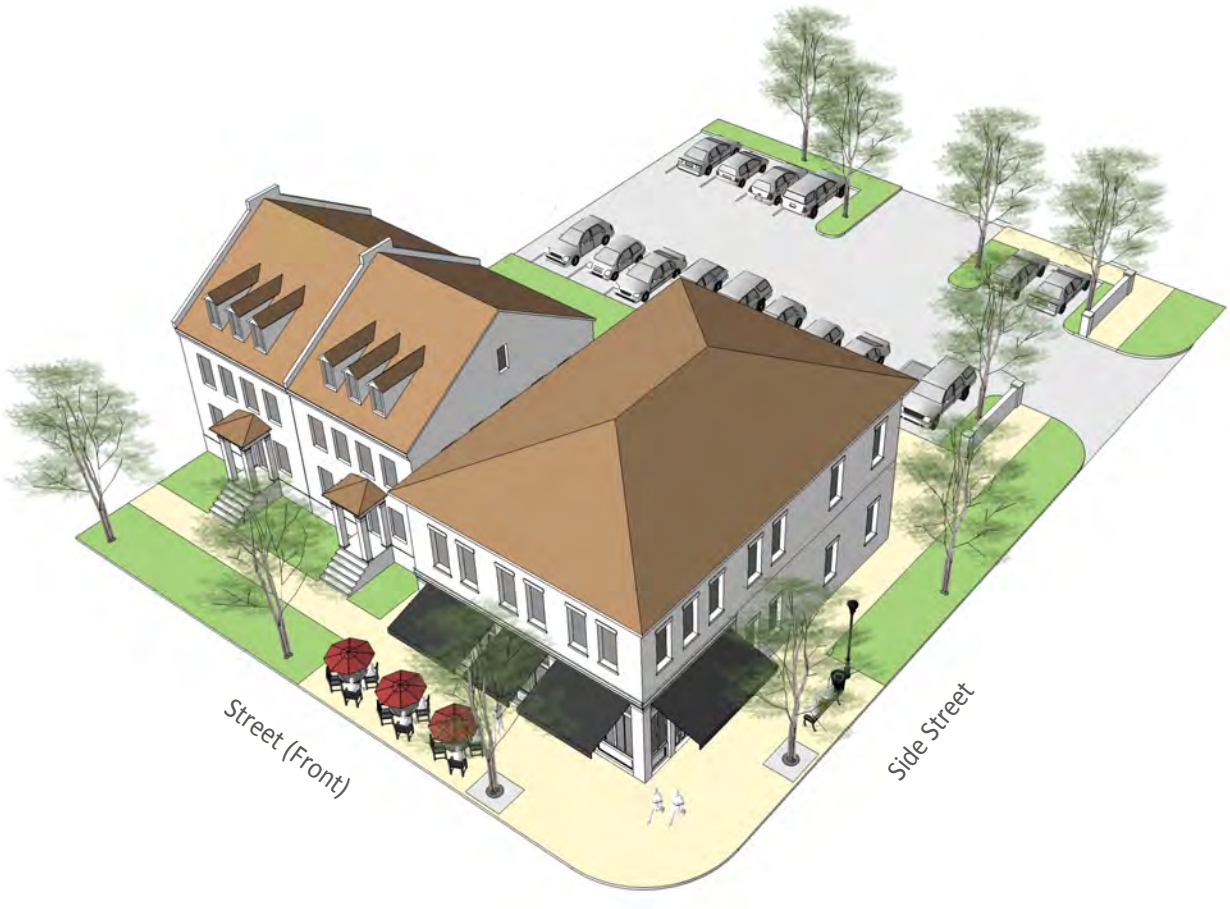
TC2

## 3-5 Town Center Corridor

### Overview

This district is located where Schodack's town center extends north along the Columbia Turnpike. Buildings are required to be street-oriented, but their placement is somewhat more flexible than in other Town Center districts, to allow adjustment to accommodate features such as wider outdoor terraces and green spaces needed.






*Illustrative example of buildings and site arrangement in the TC2 Town Center Corridor District.*

# TC2 Town Center Corridor

## Examples

Rowhouses and shopfront buildings set behind a shared public space.

Shopfront buildings with varying heights lend this street a casual yet urbane feel.

Rowhouses with at-grade ground floor entrances as well as stoops leading to second floor entries.

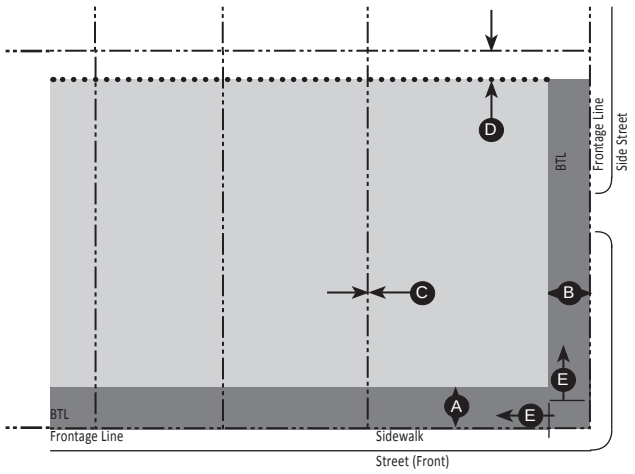
Simple architectural forms with strong, upright proportions.

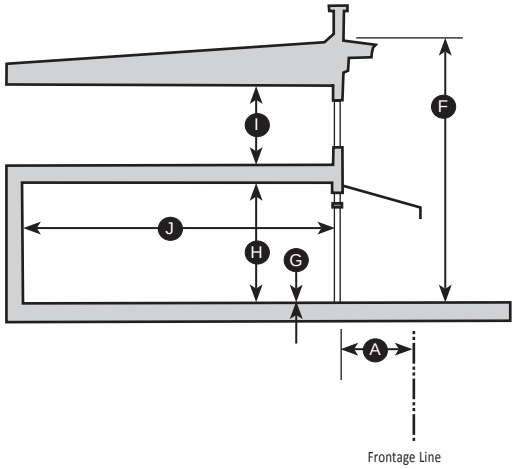
A small apartment building designed to look like a large house.

Rowhouses forming a well-defined edge to the street.

# TC2 Town Center Corridor

## Form




**Key**

Frontage/Property Line — Setback Line
Build-to Location (BTL) — Building Area

### Building Placement

#### Build-to Locations (Distance from Frontage / Property Line)

| | | |
|---|---|---|
| Front | 0' min., 20'max. | A |
| Side Street | 0' min., 20'max. | B |
| Interior Side Property Line | 0' min. | C |
| Rear | 5' min. | D |

### Frontage Buildout

| | | |
|---|---|---|
| Building Facade at BTL | | |
| Front | 50% min. | E |
| Side Street | 30% min. | |

Street Facades must be built to the BTL for the first 30' on a corner.

### Dedicated Residential Outdoor Open Space

| | | |
|---|---|---|
| Each residential unit shall be provided with unrestricted access to dedicated outdoor open space | 20 square feet min. per unit | . |
| Distance from Residential Unit | 50' max. | |
| Permitted Configurations | Balcony, Porch, Yard, Courtyard, or Roof Terrace. | |

### Building Form

#### Height

| | | |
|---|---|---|
| Main Building | 16' min.[1] | F |
| | 3 Stories max.[2] | F |
| Ground Floor Elev. Above Sidewalk | Comm. 6" max., Res. 24" min. | G |
| Ground Floor Office / Retail Ceiling | 14' min. clear | H |
| Ceiling Height | 9' min. clear | I |

[1]Measured from grade to eave or parapet base

[2]See definition of "Story" in Definitions

### Footprint

| | | |
|---|---|---|
| Depth, ground floor commercial space: | 15' min. | J |

### Allowed Frontage Types (See Sec. 3-3 Facades)

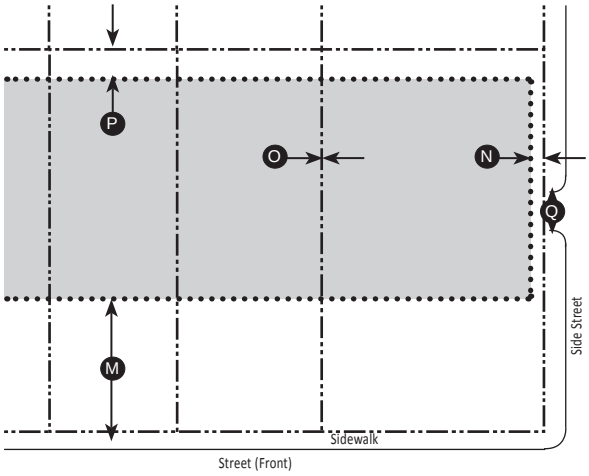
- Shopfront
- Gallery
- Forecourt
- Stoop
- Porch

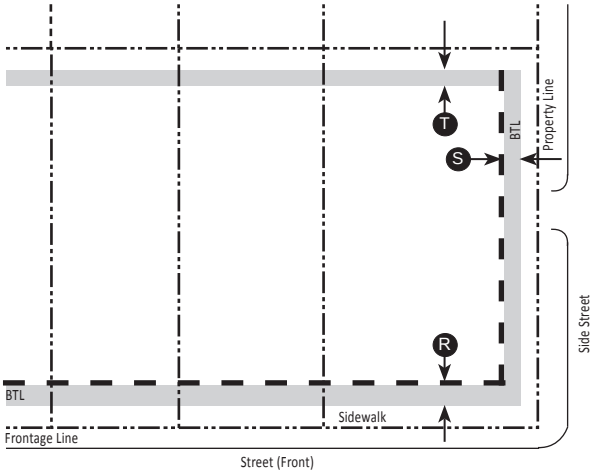
### Allowed Use Types (See Sect. 4-3 Permitted Uses Table)

| | | |
|---|---|---|
| Ground Floor | Office / Retail required within 150' of SR 9&20 Frontage Line | K |
| All Floors Otherwise | All Permitted Uses Allowed | L |

# TC2 Town Center Corridor

## Parking & Encroachments




**Key**

Frontage/Property Line
Setback Line
Parking Area

**Key**

Frontage/Property Line
Setback Line
Build-to Location (BTL)
Encroachment Area

| Parking (See Sect. 3-5 Parking Standards) | | |
|---|---|---|
| **Parking Location** (Distance from Property Line) | | |
| Front Setback | 30' min. | M |
| Side Street Setback | 30' min. | N |
| Side Setback | 0' min. | O |
| Rear Setback | 5' min. | P |

| Other Allowed Encroachments | | |
|---|---|---|
| Balconies, Bay Windows, Awnings, etc. | | |
| Front | 12' max. | R |
| Side Street | 8' max. | S |
| Rear | 4' max. | T |

| District Specific Parking Requirements | | |
|---|---|---|
| Parking Curb Cut Width | 15' max. | Q |
| No parking spaces are required for buildings that are < 500 sf | | |
| Bicycle parking must be provided and in a secure environment, except in residential buildings with 4 units or less. | | |

| Miscellaneous |
|---|
| Where a building facade steps back or is absent from the BTL, the BTL line should be maintained and defined by fence, landscape wall or hedge 30"-54" high. |
| All buildings must have a primary entrance along the front facade. |
| Loading docks, overhead doors and other service entries shall not be located on street-facing facades and shall instead be located in rear service areas |

TC3

## 3-6 Town Center General

### Overview

This district governs development near the intersection of Interstate 90 and Miller Road. While oriented primarily toward the highway, the built environment in this area is intended to retain key features of walkability.

*Illustrative example of buildings and site arrangement in the TC3 Town Center General District.*

# TC3 Town Center General

## Examples

This commercial building features a narrow band of parking.

A strong cornice line and plentiful windows lend this restaurant an urbane feel.

A small commercial building designed in the form of a cottage.

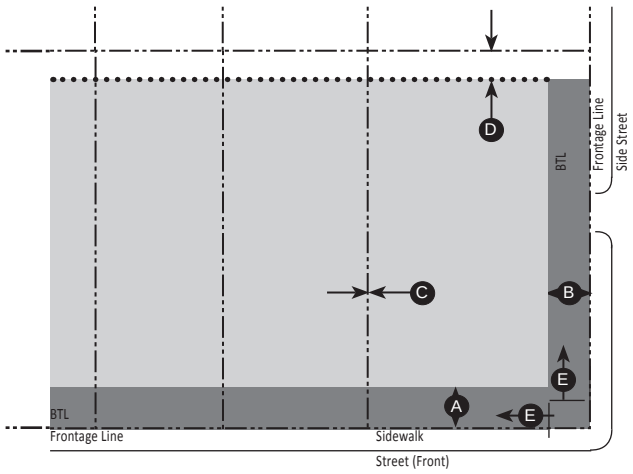
A small apartment building designed to blend with surrounding single family detached houses.

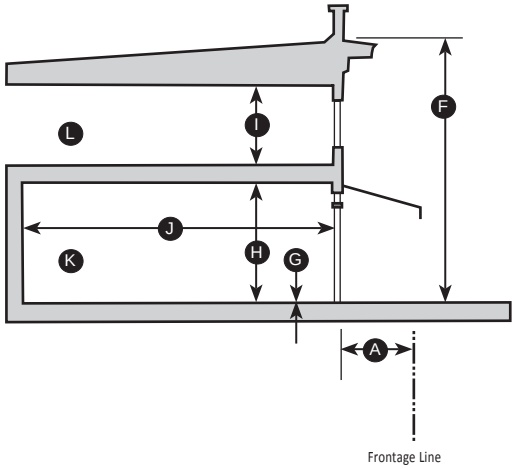
A low garden wall marks the property line.

A small office building designed as a cottage.

# TC3 Town Center General

## Form




**Key**

Frontage/Property Line — Setback Line
Build-to Location (BTL) — Building Area

### Building Placement

| Build-to Locations (Distance from Frontage / Property Line) | | |
|---|---|---|
| Front | 0' min., 75'max. | A |
| Side Street | 0' min., 75'max. | B |
| Interior Side Property Line | 0' min. | C |
| Rear | 5' min. | D |

### Frontage Buildout

| | | |
|---|---|---|
| Building Facade at BTL | | |
| Front | 30% min. | E |
| Side Street | 30% min. | |

Street Facades must be built to the BTL for the first 30' on a corner.

### Dedicated Residential Outdoor Open Space

| | | |
|---|---|---|
| Each residential unit shall be provided with unrestricted access to dedicated outdoor open space | 30 square feet min. per unit | . |
| Distance from Residential Unit | 50' max. | |
| Permitted Configurations | Balcony, Porch, Yard, Courtyard, or Roof Terrace. | |

### Building Form

| Height | | |
|---|---|---|
| Main Building | 22' min.[1] | F |
| | 3 Stories max.[2] | F |
| Ground Floor Elev. Above Sidewalk | Comm. 6" max., Res. 24" min. | G |
| Ground Floor Office / Retail Ceiling | 14' min. clear | H |
| Ceiling Height | 9' min. clear | I |

[1]Measured from grade to eave or parapet base

[2]See definition of "Story" in Definitions

### Footprint

| | | |
|---|---|---|
| Depth, ground floor commercial space: | 15' min. | J |

### Allowed Frontage Types (See Sec. 3-3 Facades)

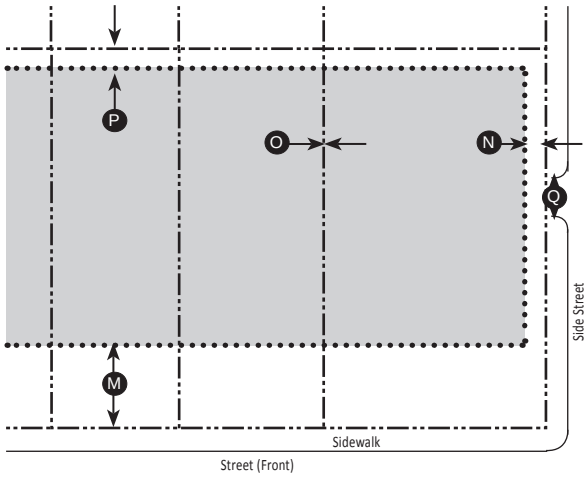
- Shopfront
- Gallery
- Forecourt
- Stoop
- Porch

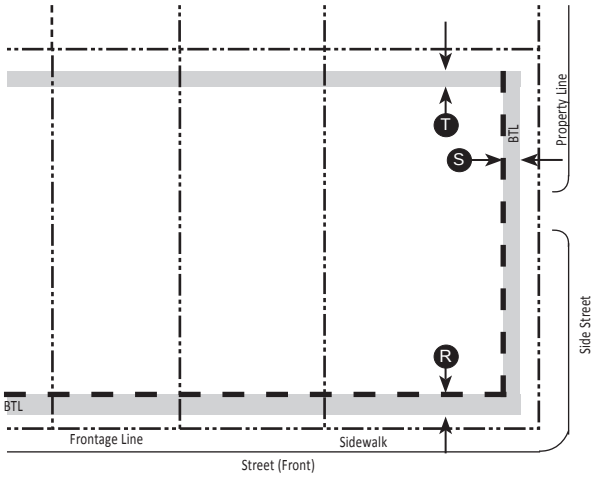
### Allowed Use Types (See Sect. 4-3 Permitted Uses Table)

| | | |
|---|---|---|
| Ground Floor | All Permitted Uses Allowed | K |
| All Floors Otherwise | All Permitted Uses Allowed | L |

# TC3 Town Center General

## Parking & Encroachments




**Key**

Frontage/Property Line
Setback Line
Parking Area

**Key**

Frontage/Property Line
Setback Line
Build-to Location (BTL)
Encroachment Area

| Parking (See Sect. 3-5 Parking Standards) | | |
|---|---|---|
| **Parking Location** (Distance from Property Line) | | |
| Front Setback | 0' min., 30' max. | M |
| Side Street Setback | 0' min., 30' max. | N |
| Side Setback | 0' min. | O |
| Rear Setback | 5' min. | P |

| Other Allowed Encroachments | | |
|---|---|---|
| Balconies, Bay Windows, Awnings, etc. | | |
| Front | 12' max. | R |
| Side Street | 8' max. | S |
| Rear | 4' max. | T |

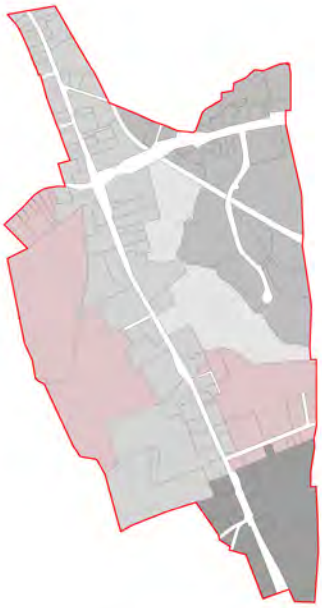
| District Specific Parking Requirements | | |
|---|---|---|
| Parking Curb Cut Width | 15' max. | Q |
| No parking spaces are required for buildings that are < 500 sf | | |
| Bicycle parking must be provided and in a secure environment, except in residential buildings with 4 units or less. | | |

| Miscellaneous |
|---|
| Where a building facade steps back or is absent from the BTL, the BTL line should be maintained and defined by fence, landscape wall or hedge 30"-54" high. |
| All buildings must have a primary entrance along the front facade. |
| Loading docks, overhead doors and other service entries shall not be located on street-facing facades and shall instead be located in rear service areas |

TC4

## 3-7 Town Center Edge

### Overview



The intent of this district is to facilitate a transition from the vibrant and urbane heart of the Schodack's Town Center to the lower intensity residential and Preserve areas on the periphery.

Buildings in this mixed-use district are predominantly residential, are set slightly further back from the street and heights and are somewhat shorter than those in the heart of the Town Center.

*Illustrative example of buildings and site arrangement in the TC4 Town Center Edge District.*

# TC4 Town Center Edge

## Examples

This fourplex building type has individual unit entrances, marked by stoops, that are accessible from the sidewalk.

Attached live-work units are a unique building type that can introduce a mix of commercial and residential uses in small increments.

A duplex contains two separate housing units.

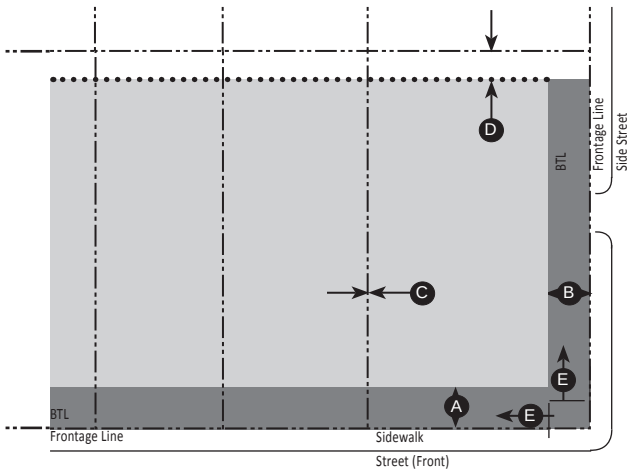
Pitched roofs help to give this mansion apartment building a comfortable scale.

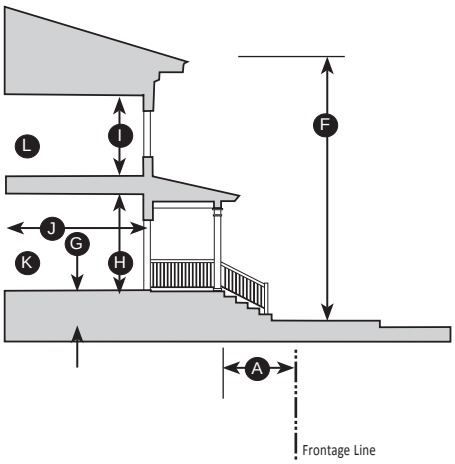
Townhouses are attached to neighboring units, forming a well-defined street edge.

A single-family house structure containing a business.

# TC4 Town Center Edge

## Form




**Key**

Frontage/Property Line — Setback Line
Build-to Location (BTL) — Building Area

### Building Placement

#### Build-to Locations (Distance from Frontage / Property Line)

| | | |
|---|---|---|
| Front | 5' min., 30'max. | A |
| Side Street | 5' min., 30'max. | B |
| Interior Side Property Line | 0' min. | C |
| Rear | 5' min. | D |

### Frontage Buildout

| | | |
|---|---|---|
| Building Facade at BTL | | |
| Front | 40% min. | E |
| Side Street | 30% min. | |

Street Facades must be built to the BTL for the first 30' on a corner.

Street Facades shall not exceed 50' in width without varying architectural expression

### Dedicated Residential Outdoor Open Space

| | |
|---|---|
| Each residential unit shall be provided with unrestricted access to dedicated outdoor open space | 40 square feet min. per unit |
| Distance from Residential Unit | 50' max. |
| Permitted Configurations | Balcony, Porch, Yard, Courtyard, or Roof Terrace. |

### Building Form

#### Height

| | | |
|---|---|---|
| Main Building | 1 Story min.[1] | F |
| | 3 Stories max.[2] | F |
| Ground Floor Elev. Above Sidewalk | Comm. 6" max., Res. 24" min. | G |
| Ground Floor Office / Retail Ceiling | 14' min. clear | H |
| Ceiling Height | 9' min. clear | I |

[1]Measured from grade to eave or parapet base

[2]See definition of "Story" in Definitions

### Footprint

| | | |
|---|---|---|
| Depth, ground floor commercial space: | 15' min. | J |

### Allowed Frontage Types (See Sec. 3-3 Facades)

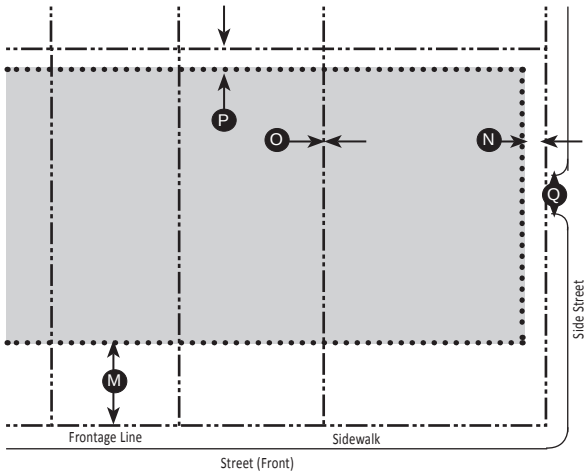
- ■ Shopfront
- ■ Gallery
- ■ Forecourt
- ■ Stoop
- ■ Porch

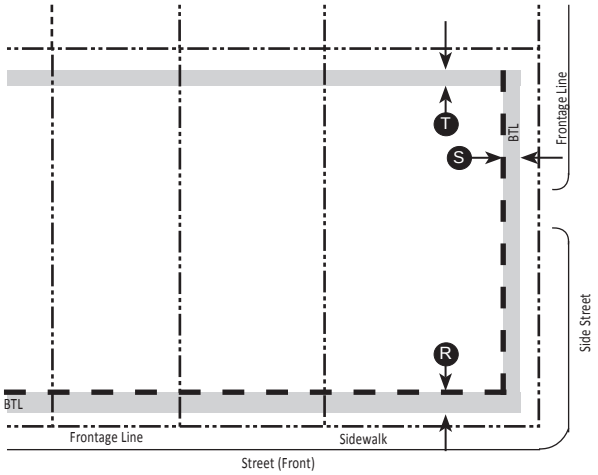
### Allowed Use Types (See Sect. 4-3 Permitted Uses Table)

| | | |
|---|---|---|
| Ground Floor | Office / Retail required within 150' of SR 9&20 Frontage Line | K |
| All Floors Otherwise | All Permitted Uses Allowed | L |

# TC4 Town Center Edge

## Parking & Encroachments




**Key**

Frontage/Property Line
Setback Line
Parking Area

**Key**

Frontage/Property Line
Setback Line
Build-to Location (BTL)
Encroachment Area

| Parking (See Sect. 3-5 Parking Standards) | | |
|---|---|---|
| **Parking Location** (Distance from Frontage Line) | | |
| Front Setback | 30' min. | M |
| Side Street Setback | 5' min. | N |
| Side Setback | 0' min. | O |
| Rear Setback | 5' min. | P |

| Other Allowed Encroachments | | |
|---|---|---|
| Balconies, Bay Windows, Awnings, etc. | | |
| Front | 12' max. | R |
| Side Street | 8' max. | S |
| Rear | 4' max. | T |

| District Specific Parking Requirements | | |
|---|---|---|
| Parking Curb Cut Width | 15' max. | Q |
| No parking spaces are required for buildings that are < 500 sf | | |
| Bicycle parking must be provided and in a secure environment, except in residential buildings with 4 units or less. | | |

| Miscellaneous |
|---|
| Where a building facade steps back or is absent from the BTL, the BTL line should be maintained and defined by fence, landscape wall or hedge 30"-54" high. |
| All buildings must have a primary entrance along the front facade. |
| Loading docks, overhead doors and other service entries shall not be located on street-facing facades and shall instead be located in rear service areas |

TC5

## 3-8 Town Center Preserve

### Overview

The preserve district consists of property that is primarily wetland. Development within this area is limited primarily due to wetland requirements, wetland buffers, and steep slopes. Limited boardwalk trails may be developed in order to access the area for recreational purposes.

*Image of the Preserve area in Schodack.*

TC5

# Town Center Preserve

## Examples

Sample boardwalks through preserved areas.

# §219-_ General Standards

## 4-1 Purpose

These general standards apply to the Town Center Districts, specifying standards for buildings that impact walkability and the quality of the public realm as well as parking requirements and the design of signage, lighting, and public open space.

## 4-2 Buildings Standards

New or substantially renovated buildings shall comply with the following:

### PRIMARY ENTRANCES

The primary entrance of every building must directly face a street or a public space. A public space may include a central garden or courtyard when that public space opens directly onto the primary street. Additional building entrances are permitted.

### ENTRY / EXIT DOORS

Public entry and exit doors which swing outward shall be recessed into the facade a minimum of three feet where the sidewalk abuts the building.

### WINDOW AND DOOR OPENINGS

Window and door openings in masonry facades should express a structural lintel above to express the conveyance of building weight. A similar method using wood trim can be used on wood-clad facades.

### COLUMNS / POSTS

The proportion of structural elements such as columns or posts should be appropriate to the weight they appear to be carrying.

## 4-3 Facades

### MATERIAL CHANGES

When materials are combined on a building façade horizontally, heavier materials should occur below lighter materials.

Changes from one material or color to another along the horizontal direction should occur at “inside corner” transitions.

Changes in material or color along the vertical direction should occur at a hard-edge “bump-out” transition which gives materials a surface to terminate into.

Facades with an overabundance of different materials or colors are generally discouraged.

### WIDE FAÇADES

Building façades longer than 150 feet shall be varied with at least one change of architectural expression.

A. These changes in expression may be a vertical element running from the ground plane to the roof, a change in fenestration, color, or texture, or a break in building façade plane or roof line.

B. These changes may be subtle or significant, but should soften the visual effect of very wide buildings, especially those directly across the street from narrower buildings.

C. Strive for an appearance of authenticity when subdividing a large façade into multiple smaller façades resembling distinct buildings.

### FAÇADE TRANSPARENCY

All building façades which face onto a street or public space shall meet the minimum transparency requirements outlined herein. The percentage of transparency per Story shall be calculated within the area between finished floor and finished ceiling and shall be a total percentage of doors and windows along that portion of the façade.

A. Minimum building façade transparency for ground Story (retail): sixty (60) percent and should allow a view of at least five (5) feet of interior space.

B. Minimum building façade transparency for ground Story (uses other than retail): thirty (30) percent

C. Minimum building façade transparency for upper Stories: thirty (30) percent

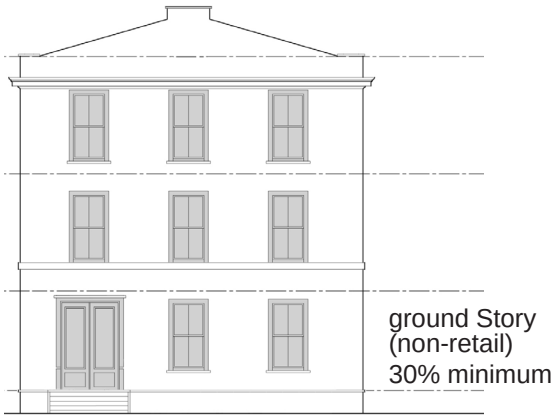
### FRONTAGE TYPES

Building frontages in new construction or to enhance existing buildings shall conform with the basic Frontage Types set forth in this section.

The illustrations and photographs provided are for illustrative purposes, and need not be interpreted literally with regard to architectural styles.

Individual descriptions and form requirements of each Frontage Type are detailed on the following pages.

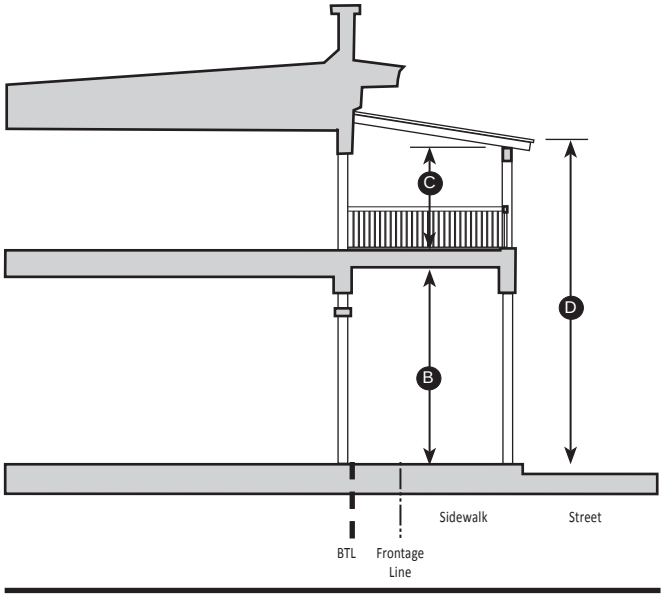
Façade transparency requirements for buildings with a retail ground Story

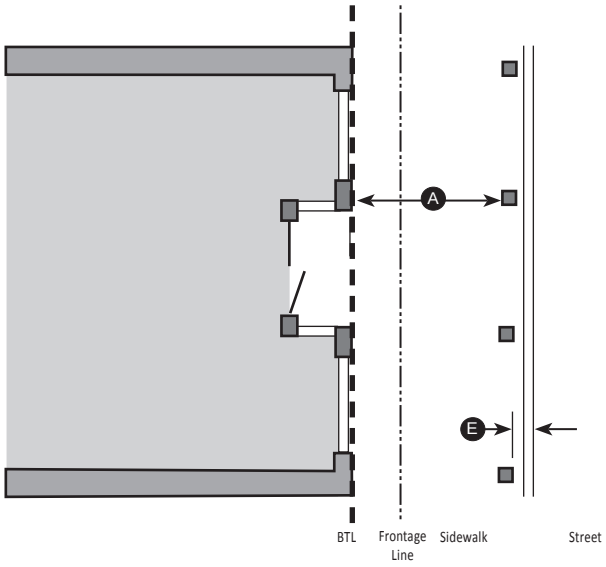



Façade transparency requirements for buildings with a non-retail ground Story

# Frontage Types:

## Gallery




**Key**

Build-to Location (BTL)

Frontage Line

### A. Description

The main façade of the building is at the Build-to Location and the Gallery element overlaps the sidewalk, eliminating the need for an awning. This Frontage Type is intended for buildings with ground-floor commercial or retail uses and may be one or two stories in height.

### B. Size

| | | |
|---|---|---|
| Depth, Clear | 8' min. | A |
| Ground Floor Height, Clear | 11' min. | B |
| Upper Floor Height, Clear | 9' min. | C |
| Height | 3 stories max | D |
| Setback from Curb | 2' min.; 3' max. | E |

### C. Miscellaneous

Galleries must also follow all the rules of the Shopfront Frontage Type.

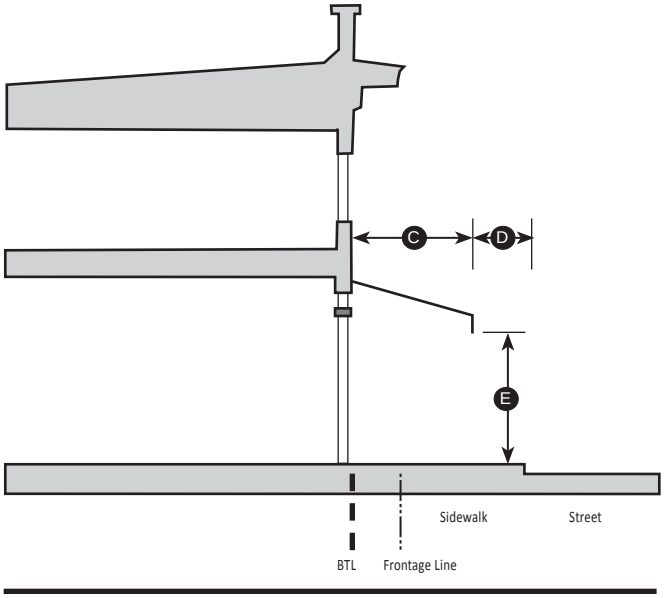
A gallery with slender metal columns.

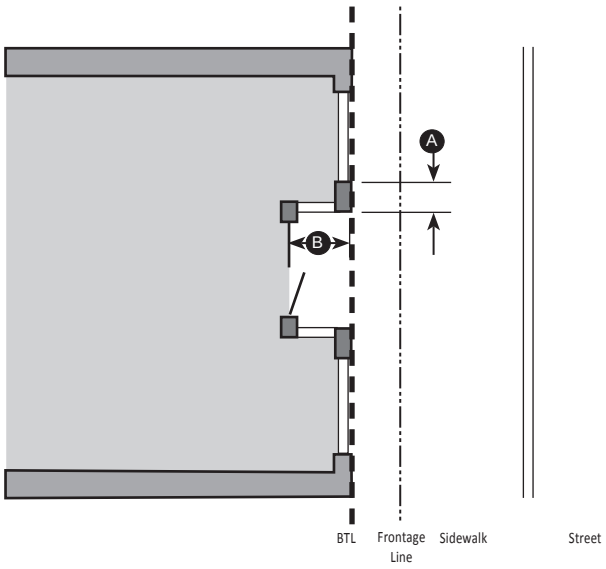
A wood framed gallery.

A masonry gallery with habitable space on the second floor.

# Frontage Types:

## Shopfront




**Key**

Build-to Location (BTL)

Frontage Line

### A. Description

The main facade of the building is at or near the frontage line and shall include a canopy or awning element that overlaps the sidewalk along the majority of the frontage. The canopy is a structural cantilevered shed roof and the awning is canvas or similar material and is often retractable.

### B. Size

| | | |
|---|---|---|
| Distance between Glazing | 2' max. | A |
| Ground Floor Transparency | See General Standards | |
| Door Recess | 5' max. | B |

### C. Canopy or Awning

| | | |
|---|---|---|
| Depth | 4' min. | C |
| Width, Cumulative | 70% of facade width min. | |
| Setback from Curb | 2' min. | D |
| Height, Clear | 8' min. | E |

### D. Miscellaneous

Doors may be recessed as long as main facade is at BTL.

Open ended awnings are encouraged.

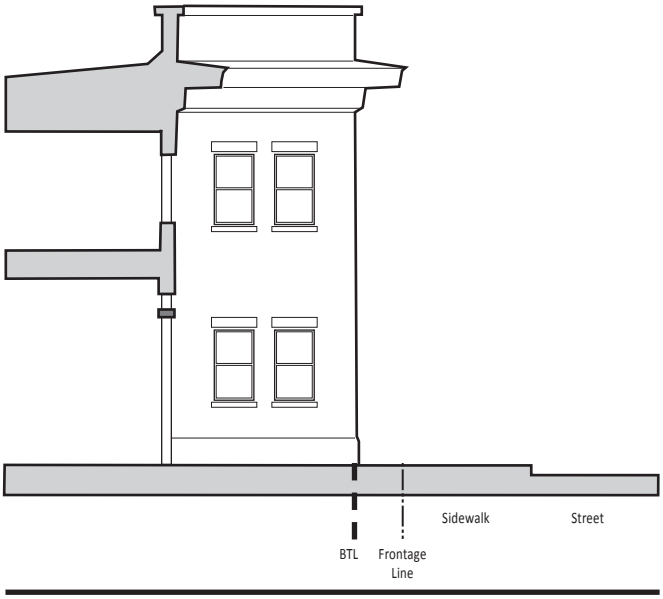
Rounded and hooped awnings are discouraged.

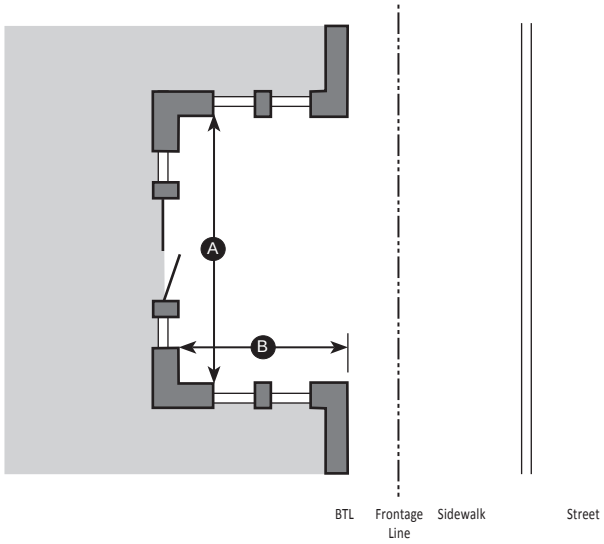
A shopfront with a recessed doorway.

A shopfront with a chamfered corner entry.

# Frontage Types:

## Forecourt




**Key**

Build-to Location (BTL)

Frontage Line

### A. Description

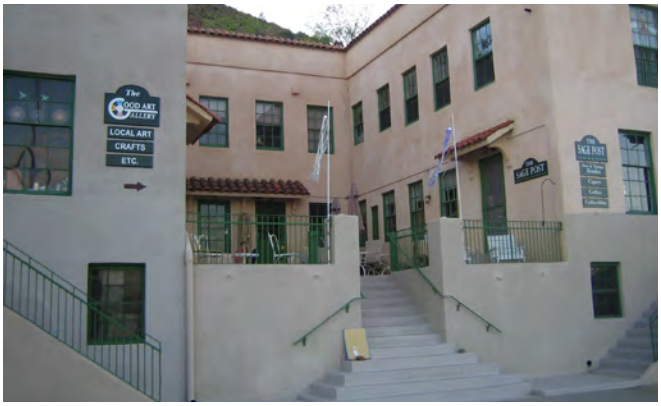
The primary portion of the building's main facade is at the Build-to Location while a small percentage is set back, creating a court space. This space can be used as an apartment entry court, garden space, or for restaurant outdoor dining.

### B. Size

| | | |
|---|---|---|
| Width, Clear | 12' min. | A |
| Depth, Clear | 12' min. | B |

### C. Miscellaneous

Forecourts are especially useful along larger, more auto-dominant thoroughfares in order to provide well-shaped, intimately sized public outdoor spaces.

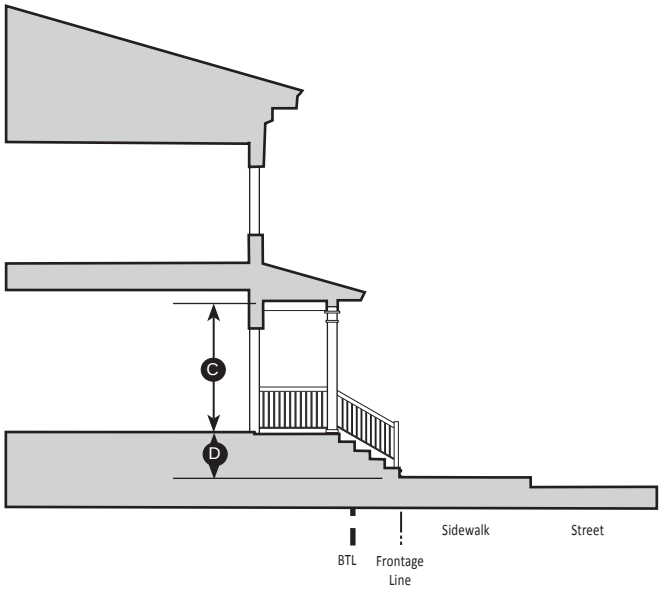


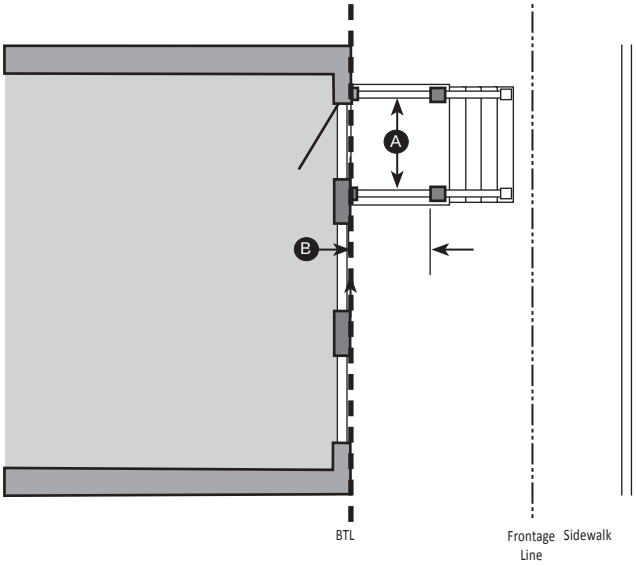

An elevated forecourt.

An elevated forecourt forming a dining terrace.

# Frontage Types:

## Stoop




**Key**

Build-to Location (BTL)

Frontage Line

### A. Description

The main façade of the building is at the Build-to Location and the elevated stoop projects forward. The stoop is used to access a first floor that is elevated above the sidewalk to ensure privacy within the building. Stairs from the stoop may descend forward or to the side.

### B. Size

| | | |
|---|---|---|
| Width, Clear | 5' min., 8' max. | A |
| Depth, Clear | 5' min., 8' max. | B |
| Height, Clear | 8' min. | C |
| Height | 2 stories max. | |
| Finish Level Above Sidewalk | 18" min. | D |

### C. Miscellaneous

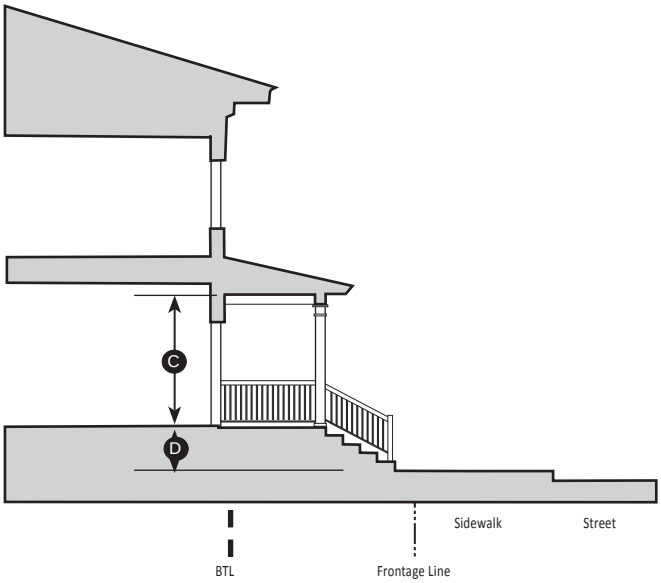
Stairs may be perpendicular or parallel to the building facade.

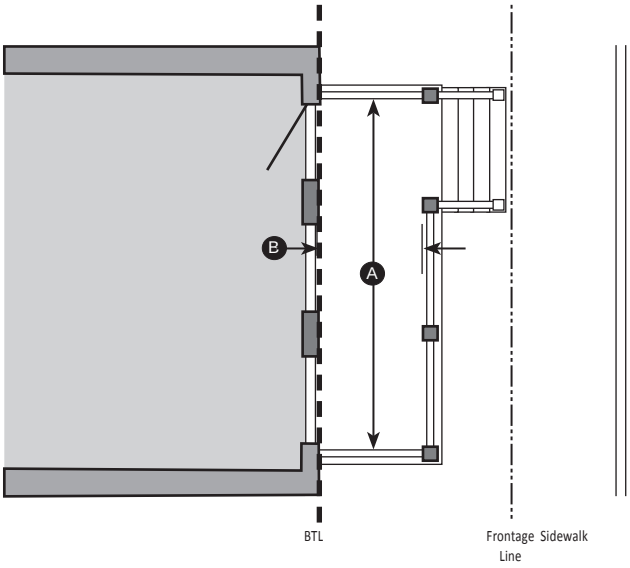
Unroofed stoops

Covered stoops

# Frontage Types:

## Porch




**Key**

Build-to Location (BTL)

Frontage Line

### A. Description

The main facade of the building is at the build-to Location and the porch projects forward. The porch is used to access a first floor that is elevated above the sidewalk to ensure privacy within the building. A porch is large enough to function as an outdoor living space.

### B. Size

| | | |
|---|---|---|
| Width, Clear | 10' min. | A |
| Depth, Clear | 8' min. | B |
| Height, Clear | 8' min. | C |
| Height | 3 stories max. | |
| Finish Level Above Sidewalk | 18" min. | D |

A 2-story porch on an apartment building.

A wraparound porch located close to the sidewalk.

## APPURTENANCES

Each building with a shopfront on the ground story is required to have awnings, balconies, colonnades, or arcades facing the primary streets. See General Standards - Frontage Types for dimensional requirements.

When appurtenances are to extend over public sidewalks, the property owner may be required to enter into a right-of-way agreement establishing the property owner's responsibility for repairing any damage that may result from public maintenance or improvements.

## ROOFS

A. Flat Roofs. Flat roof structures shall be bounded by an articulated parapet design which provides a noticeable "cap" to the building.

(1) The parapet on a single-story facade should express at least six inches in overhang depth, and be at least 18 inches tall. Parapet overhang depth and height for taller facades should be increasingly larger.

B. Sloped Roofs. Sloped roof structures are encouraged to maintain a pitch between 6:12 minimum and 12:12 maximum for all primary roof areas (not including dormers, entry canopies or similar accessory elements.)

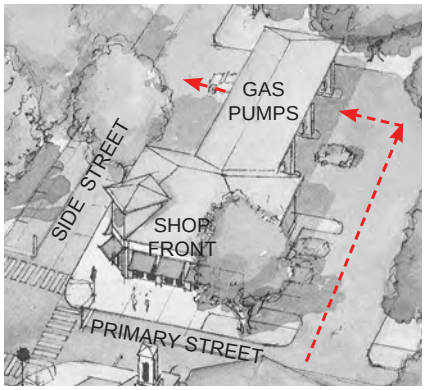
(1) Roof overhangs are encouraged to be at least 6 inches deep.

(2) Dormers and gables are encouraged along front facades to help maintain a prominent facade, reduce the scale of long runs of roof and divert rainwater and snow from entry areas.

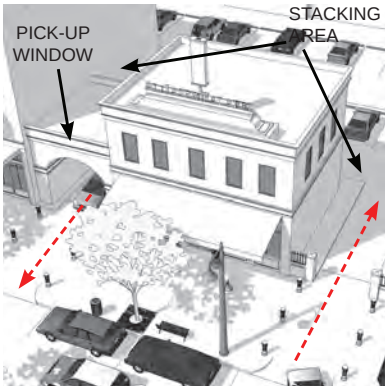
## SPECIALTY BUILDINGS

Within the Town Center Districts, it is anticipated that some automotive-oriented uses may be provided to serve the daily needs of residents. The following criteria shall be used to ensure these uses do not detract from the overall walkability of the district.

A. **Gas Stations.** A ground-floor shopfront shall face the street and define the frontage of the lot. All pumps, parking, and drive-through areas must be located behind the shopfront building.




**B. Drive-thru.** A ground-floor shopfront shall face the Primary Street. All parking shall be located in the rear and accessed from a rear alley when present. Drive-thru windows shall be located to the side or rear of the building.



## 4-4 Site Standards

### SERVICE AREAS & LOADING DOCKS

Trash and recycling dumpsters or similar collection areas shall be located in the rear or to the side of buildings and screened from view from adjacent public right-of-ways, properties and pedestrian walkways (not including service alleys).

### MECHANICAL EQUIPMENT

Mechanical equipment, including roof-mounted, facade mounted, or ground level mounted, shall be screened from view from adjacent public spaces, (does not include alleys).

Screening shall be achieved with non-deciduous landscape plantings, architectural building elements or parapet walls.

## 4-5 Parking Standards

The parking requirements of this section apply to all properties applying for a permit for the construction, renovation, addition, site plan alteration, or change of use.

The intent of the parking standards is to encourage a balance between pedestrian-oriented development and necessary car storage for the Town Center. The goal is to construct neither more nor less parking than is needed.

### PARKING REQUIREMENTS

A. Parking shall be provided for each use based upon the minimum and maximum requirements outlined by use in the Parking Requirement Chart.

B. Parking may be located on the same lot as the use it serves. Required parking may also be located on-street, provided the on-street parking space is directly adjacent to the business within a street, or in a non-adjacent parking lot, provided the space is within a quarter (1/4) mile of the building's front door.

C. A minimum of one bicycle parking space within a bicycle rack shall be provided for every three vehicular spaces.

D. Shared and reduced parking is encouraged in the Town Center. The minimum number of parking spaces required for uses may be reduced with Planning Board approval, if the reduction for these uses can be justified through a parking analysis.

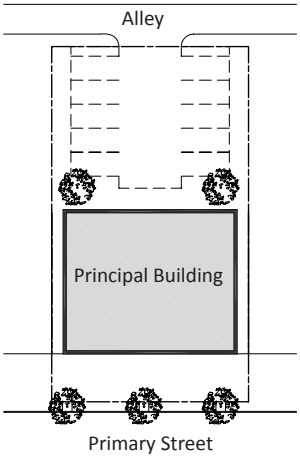
| Parking Requirement Chart | |
|---|---|
| Use | Max # of Spaces Permitted in TC1; **Min # of Spaces Required TC2-TC5:** |
| Residential | 1 per dwelling unit in mixed-use buildings<br>1.25 per dwelling unit in multi-family residential buildings<br>2 per single or two family dwelling |
| Lodging | 1 per guest room |
| Office | 3 per 1,000 sq. ft. of office space |
| Retail | 3 per 1,000 sq. ft. of retail space |
| Civic | TBD by the Town of Schodack |
| Education | 1 per 12 students |
| Automotive / Transportation | 2 per 1,000 sq. ft. of floor area |
| Other: General | TBD by the Town of Schodack |
| Other: Agricultural / Industrial | 1 per employee on largest shift |

### PARKING ACCESS

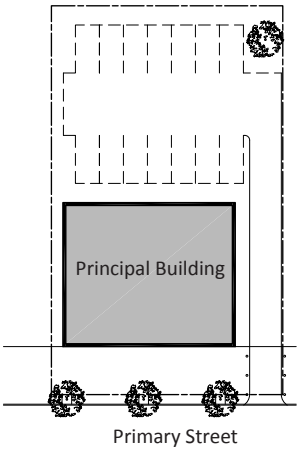
A. All parking shall be accessed from rear alleys where they exist and/or from side streets if the lot is located on a corner. If no rear alley or side street exists, then efforts should be made to gain access across neighboring properties.

B. When access to rear parking must be directly from the primary street, driveways shall be located along the sides of the property lines and designed such that pedestrians crossing on sidewalks always have the right-of-way.

## 4-6 Signage Standards




Parking: access by alley




Parking: access by street

See Signage Table within the Code for Signage Standards within the Town Center.

## 4-7 Lighting Standards

Adequate and quality lighting of the sidewalk and street area is essential to creating a safe and inviting streetscape. In addition to Town of Schodack regulations regarding lighting, the following standards and guidelines shall also apply to Town Center Districts.

### GENERAL LIGHTING STANDARDS

Lighting fixtures shall be appropriately chosen for the Town Center. There shall also be consistency within the Town Center in creating a unifying scheme of illumination that is appropriate to the scale of the street and the level of evening activity. Lamp styles should not be mixed along any one particular block of a street.

Light fixtures shall be downcast or low cut-off fixtures to prevent glare and light pollution.

In order to conserve energy and reduce long-term costs, energy-efficient lamps shall be used for all public realm lighting.

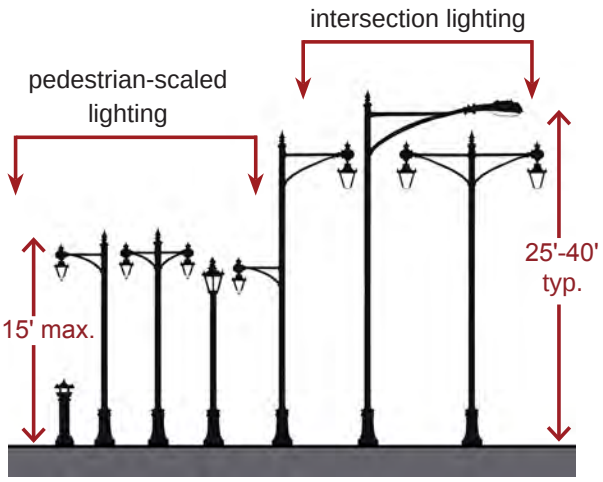
### STREET LIGHTING

A combination of pedestrian-scaled street light fixtures and intersection street light fixtures may be required to ensure a well-lit street area and to establish a unifying element along the street.

Pedestrian-scaled fixtures shall be used on all streets in Town Center District frontage areas.

Intersection-scaled lighting may be used in addition to pedestrian-scaled lights where required by the Town of Schodack.

Street lights shall be aligned with street tree placement (generally between two and a half (2.5) feet and four (4) feet from the back of the curb). Placement of fixtures shall be coordinated with the organization of sidewalks, landscaping, street trees, building entries, curb cuts, signage, etc.



Pedestrian and Intersection Lighting

The height of light fixtures shall be kept low (generally not taller than fifteen (15) feet to promote a pedestrian scale to the public realm and to minimize light spill to adjoining properties. Light fixtures shall be closely spaced (generally not more than thirty (60) feet on center) to provide appropriate levels of illumination.

Light poles may include armature that allows for the hanging of banners or other amenities (e.g., hanging flower baskets, artwork, etc.).

### PARKING LOT LIGHTING

A. All fixtures shall be full cutoff, downward facing.

B. Light fixtures located within the interior asphalt area of a parking lot shall not exceed 30 feet in height. Light fixtures located along the perimeter edge of a parking area within 50 feet of a property line shall not exceed 15 feet.

### PEDESTRIAN WALKWAY LIGHTING

A. Light fixtures located along pedestrian walkways adjacent to parking lots shall not exceed 15 feet in height.

B. Light fixtures located along internal pedestrian walkways or paths not adjacent to a parking area shall not exceed 10 feet in height.

### BUILDING AND SECURITY LIGHTING

A. All exterior building or security lighting must be full cutoff, shielded and/or angled downward to focus the light only on the intended doorway or walkway as necessary.

B. Security lighting is encouraged to be provided with regular pedestrian light fixtures where visible from the street or public way to match others used on site.

C. Building mounted architectural "accent lights" are encouraged to emphasize architectural character and signage.

D. Business owners are encouraged to assist with lighting the sidewalk and to accent their business location by leaving display window and interior lighting on at night. Lighting shall be designed in such a way as to prevent the direct view of the light source to neighboring residential areas.

# §219-_ Future Streets and Civic Spaces

## 5-1 Future Streets

### GENERAL STANDARDS

Street configurations, public or private, shall be as designated on the Future Streets and Civic Spaces Map or as otherwise approved.

The precise location and alignment of new streets may be adjusted to allow flexibility in the design of the site plan, but the number of connections provided to surrounding parcels shall not be reduced.

The design of new streets and modifications to existing streets shall adhere to the following requirements:

A. Improvements to existing thoroughfares shall be coordinated with the Planning and Zoning Director. Some dimensional flexibility is permitted for street types to account for varying ROW widths, however they shall be designed to have all the basic functional characteristics including roadway width, on street parking, sidewalks, trails, street trees and landscaped areas shown for their type.

B. All streets shall connect to other streets.

C. Cul-de-sacs, and T-turnarounds are not permitted.

D. Dead-end streets are only permitted for those shown on the Future Streets and Civic Spaces Map when the adjacent property has not been developed or redeveloped.

E. All streets shall have at least two (2) travel lanes, one (1) in each direction; however, streets around squares may have one (1) travel lane with one-way traffic.

F. Where possible, there shall be parking lanes which in addition to on-street parking may be used for “drop off” areas or bus stops.

G. On-street parking lanes shall not be closer than twenty-five (25) feet to intersections measured from the curb line.

H. All streets shall have sidewalks which are a minimum width of five (5) feet, and have a continuous unobstructed path of a width no less than sixty (60) inches. This path shall be unobstructed by utility poles, fire hydrants, benches or any other temporary or permanent structures.

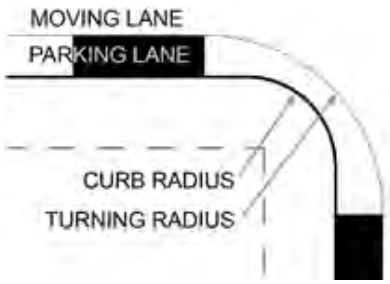
I. With the exception of fire hydrants, utilities shall run underground and above-ground projections of utilities shall be placed in rear service areas wherever practicable.

### CURB RADIUS

Corner curb radius designs fall into two distinct categories: corners with, and without, on-street parking.

A. Streets with on-street parking shall have curb radii of fifteen (15) feet maximum. Tight curb radii inhibit drivers from turning corners at high speeds, enhancing safety for pedestrians. The effective turning radius is larger than the curb radius when parking is present. Thus, the effective turning radius can be thirty (30) plus feet when the curb radius is fifteen (15) feet.

B. Corners without parallel parking require the curb radii to be similar to the turning radii, with the curb radius between twenty (20) feet and thirty (30) feet.




### REAR SERVICE LANES

Where possible, rear service lanes shall be used for access to parking and services at the rear of lots. See Rear Service Lane Chart for required dimensions.

A. Rear service lanes providing access to residential buildings shall be built to residential standards.

B. Rear service lanes providing access to non-residential uses shall be built to non-residential standards.

C. Where a rear service lane provides access to a block with both residential and non-residential uses, it shall be built to a non-residential standard.

D. Rear service lanes should meet streets with a gutter pan, allowing the sidewalk to continue uninterrupted across a drive.

| Rear Service Lane Type | Right-of-Way | Pavement Width |
|---|---|---|
| Residential | 20' Min. | 12' - 18' |
| Non-Residential | 20' Min. | 18' - 24' |

## 5-2 Civic Spaces

### GENERAL STANDARDS

Civic space in the form of parks, greens, squares, plazas, playgrounds, or community gardens shall at a minimum be located according to the Future Streets and Civic Spaces Map. All designated civic spaces shall be at grade level and shall be accessible to the public.

Location and size of the designated civic spaces shall conform with the Civic Open Space Standards.

Squares and plazas shall be densely shaded and provide seating. Trees and shrubs shall be of sufficient quantity and located as to define a specific geometry of open space and shall promote security by allowing visibility through all areas.

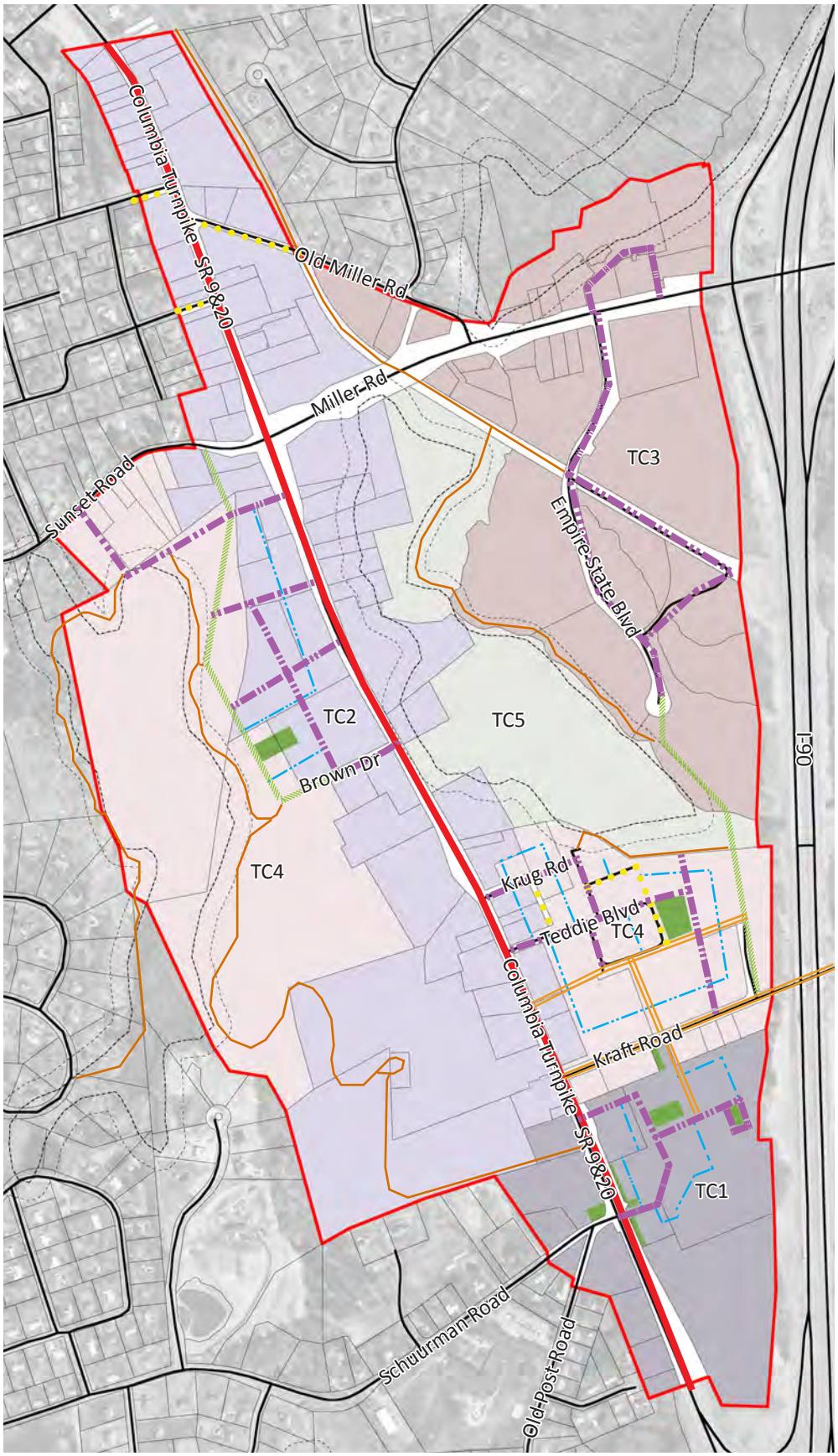
Ground surface shall be a combination of paving, lawn or ground cover integrated in design with trees and shrubs. Fountains, sculpture, and works of art are encouraged. Street furniture shall not obstruct sight visibility triangles at street intersections.

New civic open spaces may be of the following types, which are allowable in various Town Center Districts as indicated by the letter "X" in the following table:

| Civic Space Type | Must Front at Least: | Typical Size | Town Center District | | | |
|---|---|---|---|---|---|---|
| | | | TC1 | TC2 | TC3 | TC4 |
| Park | 1 street | min. 8 acres | | | x | x |
| Green | 2 streets | 0.5 to 5 acres | | x | x | x |
| Square | 3 streets | 0.5 to 2 acres | x | x | x | |
| Plaza | 1 street | 0.1 to 2 acres | x | | | |
| Playground | 0 streets | 0.1 to 1 acre | x | x | x | x |
| Community Garden | 0 streets | 0.1 to 1 acre | x | x | x | x |

## 5-3 Future Streets and Civic Spaces Map

- TC1: Town Center Core
- TC2: Town Center Corridor
- TC3: Town Center General
- TC4: Town Center Edge
- TC5: Preserve
- Town Center Form-Based Code Boundary
- Parcels
- Wetland
- Wetland Buffer
- Existing Road
- Neighborhood Street, Type 1
- Neighborhood Street, Type 2
- Neighborhood Street, Type 3
- Edge Street
- SR 9&20
- Potential Alley Segment
- Potential Trail Segment
- Designated Civic Space Location



## 5-4 Future Street Types

### Neighborhood Street, Type 1

| Application | |
|---|---|
| Movement Type | Slow |
| Design Speed | 20 mph |
| Pedestrian Crossing Time | 10.2 seconds |

| Overall Widths | | |
|---|---|---|
| Right-of-Way (ROW) Width | 60' - 70' | A |
| Curb Face to Curb Face Width | 36' | B |

| Lanes | | |
|---|---|---|
| Traffic Lanes | 10' | C |
| Bicycle Lanes | sharrows | |
| Parking Lanes | 8' | D |
| Medians | n/a | |

Note: On-street parking may be replaced with bicycle lanes on one or both sides with approval.

| Edges | |
|---|---|
| Curb Type | 6" Raised |
| Planter Type | 4'x4' tree well adjacent to curb |
| Landscape Type | Medium Trees, evenly spaced @ 30' o.c. avg. |
| Walkway Type | 12' - 17' sidewalk. (A 6' wide tree lawn may be incorporated adjacent to the curb in TC2, TC3, TC4 & TC5 zones). E |
| Lighting | Aligned with street trees |

| Intersection | |
|---|---|
| Curb Radius | 10' |
| Distance Between Intersections | 100' min. See Regulating Plan |

Note: Building placement and form requirements vary depending on Town Center District. F

# Neighborhood Street, Type 2

| Application | |
|---|---|
| Movement Type | Slow |
| Design Speed | 20 mph |
| Pedestrian Crossing Time | 9.7 seconds |

| Overall Widths | | |
|---|---|---|
| Right-of-Way (ROW) Width | 70' - 80' | A |
| Curb Face to Curb Face Width | 34' | B |

| Lanes | | |
|---|---|---|
| Traffic Lanes | 10' | C |
| Bicycle Lanes | 6' sidewalk cycle track | D |
| Parking Lanes | 7' | E |
| Medians | 6' | F |

Note: Both bicycle lanes may be grouped together on one side of the street with approval.

| Edges | | |
|---|---|---|
| Curb Type | 6" Raised | |
| Planter Type | 4'x4' tree well | |
| Landscape Type | Medium Trees, evenly spaced @ 30' o.c. avg. | |
| Walkway Type | 6' - 11' sidewalk | G |
| Lighting | Aligned with 4'x4' tree wells | |

| Intersection | |
|---|---|
| Curb Radius | 10' |
| Distance Between Intersections | 100' min. See Regulating Plan |

Note: Building placement and form requirements vary depending on Town Center District. G

# Neighborhood Street, Type 3

| Application | |
|---|---|
| Movement Type | Slow |
| Design Speed | 20 mph |
| Pedestrian Crossing Time | 8 seconds |

| Overall Widths | | |
|---|---|---|
| Right-of-Way (ROW) Width | 48' - 60' | A |
| Curb Face to Curb Face Width | 28' | B |

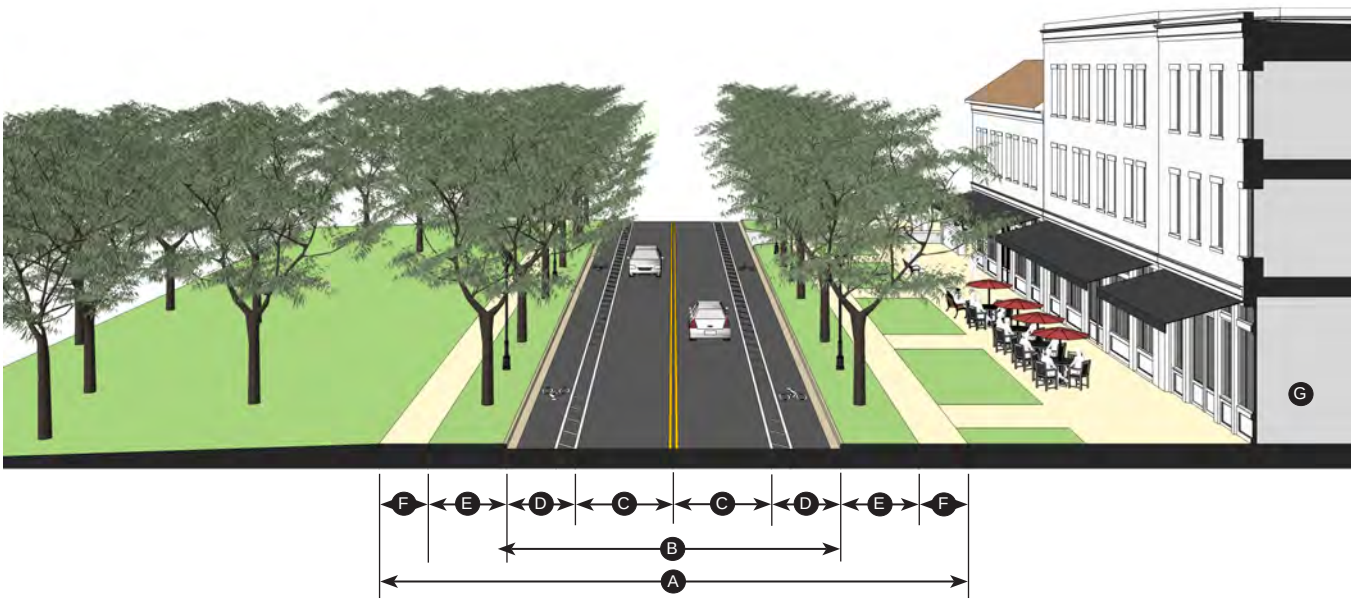
| Lanes | | |
|---|---|---|
| Traffic Lanes | 10' | C |
| Bicycle Lanes | sharrows | |
| Parking Lanes | 8' | D |
| Medians | n/a | |

| Edges | | |
|---|---|---|
| Curb Type | 6" Raised | |
| Planter Type | 4x4 tree well | |
| Landscape Type | Medium Trees, evenly spaced @ 30' o.c. avg. | |
| Walkway Type | 10' - 16' sidewalk | E |
| Lighting | Aligned with 4'x4' tree wells | |

| Intersection | |
|---|---|
| Curb Radius | 10' |
| Distance Between Intersections | 100' min. See Regulating Plan |

Note: Building placement and form requirements vary depending on Town Center District. F

# Edge Street




| Application | |
|---|---|
| Movement Type | Slow |
| Design Speed | 30 mph |
| Pedestrian Crossing Time | 10.2 seconds |

| Overall Widths | | |
|---|---|---|
| Right-of-Way (ROW) Width | 60' - 70' | A |
| Curb Face to Curb Face Width | 34' | B |

| Lanes | | |
|---|---|---|
| Traffic Lanes | 10' | C |
| Bicycle Lanes | 7' buffered bike lane* | D |
| Parking Lanes | n/a | |
| Medians | n/a | |

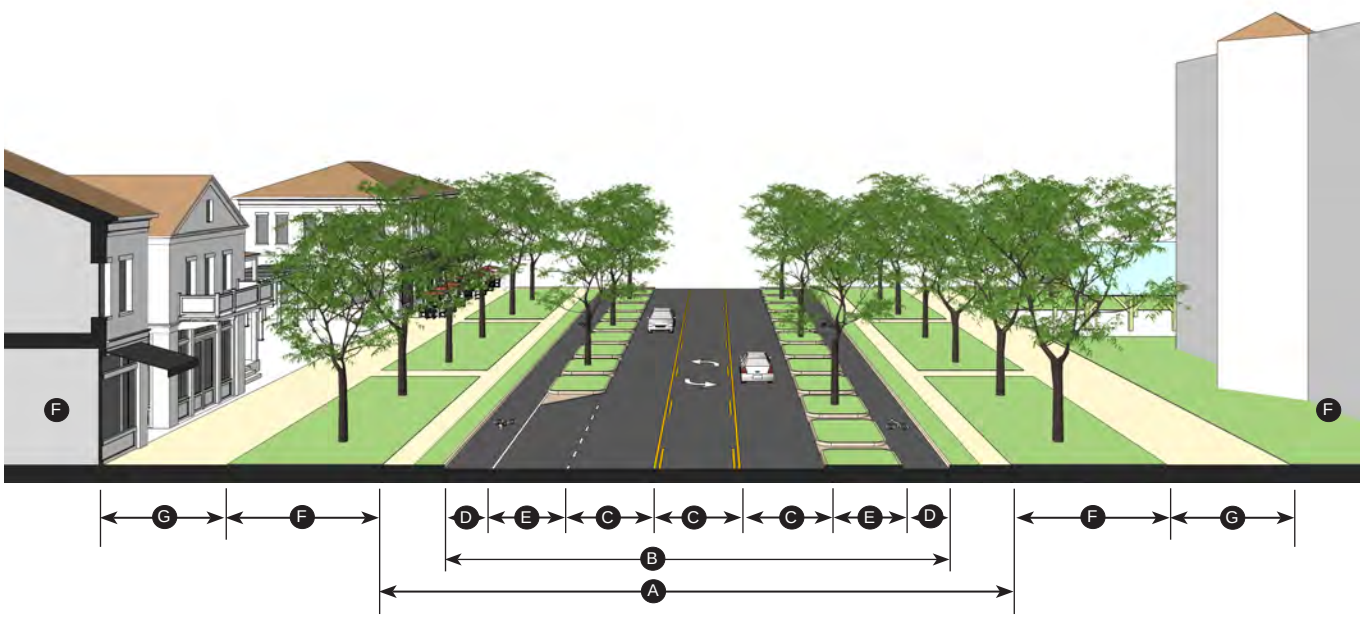
| Edges | | |
|---|---|---|
| Curb Type | 6" Raised | |
| Planter Type | 6'-8' swale | E |
| Landscape Type | Medium Trees, evenly spaced @ 40' o.c. avg. | |
| Walkway Type | 6-8' sidewalk | F |
| Lighting | Within 6'-8' swale | |

| Intersection | |
|---|---|
| Curb Radius | 15' |
| Distance Between Intersections | 200' min. See Regulating Plan |

Note: Building placement and form requirements vary depending on Town Center District. G

* The 7' buffered bike lane may alternatively be replaced with a 7' wide lane of parallel on-street parking, and either a travel lane sharrow or a cycle track.

# SR 9&20




| Application | |
|---|---|
| Movement Type | Medium |
| Design Speed | 35 mph |
| Pedestrian Crossing Time | 15 seconds |

| Overall Widths | | |
|---|---|---|
| Right-of-Way (ROW) Width | 62'-81' | A |
| Curb Face to Curb Face Width | 46'-65' | B |

| Lanes | | |
|---|---|---|
| Traffic Lanes | 11' | C |
| Bicycle Lanes | sharrows | D |
| Parking Lanes | n/a | |
| Medians | 10' | E |

| Edges (located within easement adjacent to R.O.W.) | | |
|---|---|---|
| Curb Type | 6" Raised | |
| Planter Type | 30' swale | F |
| Landscape Type | Medium Trees, evenly spaced @ 30' o.c. avg. | |
| Walkway Type | 16' sidewalk min. | G |
| Lighting | Aligned with trees | |

| Intersection | |
|---|---|
| Curb Radius | Existing Configuration |
| Distance Between Intersections | Existing Configuration |

Note: Building placement and form requirements vary depending on Town Center District. F

## 6 Definitions

**Appurtenances.** Porches, balconies, patios, seating areas, canopies awnings, etc., which extend outward from the facade of a building which do not count as an extension of the facade itself for the purposes of measuring setbacks and build-to locations.

**Building Height.** Building height is measured from the average front facade grade level to the mean height between the eaves and the roof peak (for sloped roof structures) and to the top of the parapet (for flat roof structures.)

**Build-To Location.** The specific location or flexible area where the facade of a building must be located, measured as both a minimum and maximum setback distance from the Frontage Line.

**Civic Space.** A natural or landscaped outdoor area provided for the purpose of active or passive public recreation. May include publicly accessible outdoor amenities such as a playground, seating area, picnic area, multi-use path and temporary or permanent small outdoor performance space or religious facility.

**Facade Transparency.** The amount of transparent window glass or other openings in the facade of a building, relative to the overall surface area of the facade. Facade transparency is measured separately for the ground floor levels and upper floor levels. The ground floor area is measured between 2 feet above the ground to 12 feet above the ground. Facade transparency for upper floors is measured from second finished floor level to the ceiling of the topmost floor.

**Frontage Line.** The front of a presumed property where a development parcel or property meets with either a pubic or private right-of way, street, or public space and is where the front of a building is to be located.

**Frontage Buildout.** The percentage of the lot width which must be occupied by building facade along the Build-To Location. For example, a property which is 100 feet wide with a frontage width percentage of 60% would require that at least 60 feet of facade length be maintained in the build-to location. Any additional length of front facade would be allowed to step back further from the frontage line, if desired. The intent of this requirement is to encourage development to maximize their front facade exposure along the street or public space.

**Regulating Plan.** A map or set of maps that shows the physical locations and boundaries of regulatory items such as Zone Districts, Future Streets, and Civic Spaces subject to regulation by this Code.

**Setbacks.** The minimum distance a building facade or parking area must be located from a frontage line or public right-of-way. Similar to a Build-To Location, except the building or parking can be located anywhere behind that line.

# Appendix A: Public Participation

PENDING RELEASE: Immediate
FOR MORE INFORMATION CONTACT: Town Offices: (518) 477-7938

# Public Announcement – Town of Schodack
# Town Center Form-Based Zoning for Route 9/20!

---

**When: Wednesday, May 10, 2016 at 7 PM**
**Where: Town Hall, 2nd Floor**

Please join the Town of Schodack during the upcoming public meeting **May 10th at 7 PM at the Town Hall, 2nd Floor**. The Town of Schodack is in the process of developing the Town Center Form-Based Zoning Code for Route 9/20. These regulations are intended to transform the area from an auto-dominated environment to one that incorporates a mix of uses, walkable streets, compactness, density, good design, useable open space, and appropriate parking. During our meeting, we will share information about:

- What is a form based code?
- What is a regulating plan?
- What are the Town Center's Street types?
- How will it be used in Schodack?
- How will the proposed zoning affect your property?

The public will have an opportunity to share feedback on specific code elements, such as but not limited to:

- Density
- Height
- Uses
- Streetscape
- Parking
- Signage

All residents, property owners, civic groups, business owners, community leaders, and all other interested stakeholders are welcome to participate. Provide your feedback on the Town Center Form-Based Zoning plan for Route 9/20. There will be a presentation of the Plan's highlights and then an opportunity to provide feedback.

If you have any questions, please contact the Town Offices at (518) 477-7938.

Nadine Fuda, Planning & Zoning Director
**Town of Schodack**
**nadine.fuda@schodack.org**
**(518) 477-7938**
**www.schodack.org**

Laberge Group

ENGINEERING • ARCHITECTURE • SURVEYING • PLANNING

# Town Center Form Based Zoning Project

***KICK-OFF MEETING***

**TOWN OF SCHODACK, Town Hall**

**FEBRUARY 18, 2016**

**9:00 a.m. – 10:30 a.m.**

Agenda

| | | |
|---|---|---|
| **I.** | **Introductions**<br>• Consultant & Steering Committee Member Introductions | **10 Minutes** |
| **II.** | **Project Scope of Work**<br>• Laberge Group – Summary of Previous Planning Work<br>• CDTC – Program Update<br>• Project Purpose, Goals and Objectives<br>• Review Scope of Work & Timeline<br>• Committee Perspectives<br>• Roles, Expectations & Implementation | **35 Minutes** |
| **III.** | **Public Participation**<br>• Set Study Area Visit<br>• Set Public Workshop Schedule<br>• Review/Discuss Stakeholders & Focus Groups<br>• Discuss Community Outreach Methodologies | **35 Minutes** |
| **IV.** | **Next Steps**<br>• Set Committee Meeting #2<br>• Prepare for first charrette & stakeholder interviews | **10 Minutes** |

4 Computer Drive West • Albany, New York 12205 • (518) 458-7112 • Fax (518) 458-1879
www.labergegroup.com

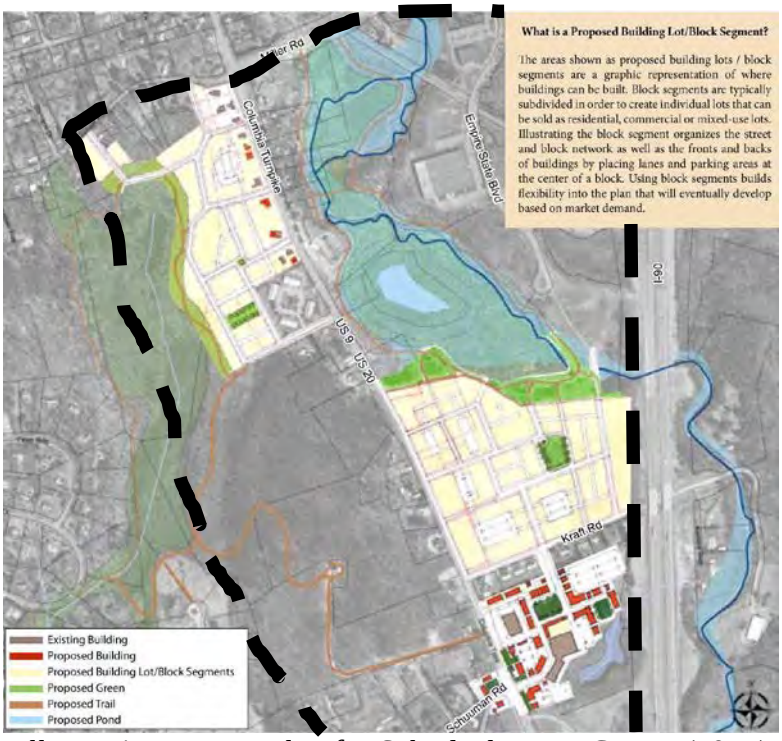
**SCHODACK TOWN CENTER FORM BASED ZONING CODE**
**BACKGROUND INFORMATION AND SCOPE OF WORK**

## Study Purpose

The Town of Schodack, in partnership with the CDTC,desires new zoning code regulations that incorporate Town Center design techniques as identified in the *Schodack Town Center Plan (2014)*. The Illustrative Master Plan for the Schodack Town Center is shown on page 2 and will serve as the basis for the development of a Form Based Zoning Code. The new zoningwill be coordinated with the Town's current effort to update its town wide zoning. Form Based zoningallows for theidentification ofTown Center design guidelines and standards, allows for the development of a detailed streets plan that integrates Complete Streets principles and offersdeveloperspredictability regarding what is desired by the community.

## Study Area

The outerboundary of the Town Centerextends approximately fromMiller Road on the north(which provides access toExit 10 of I-90) to just south of Schuurman Road. The widthof the corridor varies and isbounded by I-90 on the eastand an area about one mile west of US 9/20. (See page 4 for the general study area boundary as it relates to the existing zoning). The TownCenter's core is primarily thearea of US 9/20 adjacent toTown Hall. The illustrative plans in the *Schodack Town Center Plan (2014)*will serve as the basis for implementable zoning amendments.Multi-modal transportation connections in the study area are essential to promote walking and biking throughout the Town Center.



*Illustrative Master Plan for Schodack Town Center (2014)*

## Study Background

This effort will build upon the Town of Schodack's *Comprehensive Plan*, the*Route 9 Corridor Development Plan* and most importantly, the recently completed *Schodack Town Center Plan*.Currently, the Town is served by three interstate interchanges and is generally identified by the land use surrounding each interchange rather than by quality of life, look, or overall vision. The 2014 *Schodack Town Center Plan*used an extensive public involvement process to develop a vision for the Town Center, in concept, and to outline a path toward implementation.

The vision for the Town Center focuses on five key principles:

Nodes of Development: Two nodes of development will define the Town Center, one at the intersection of US 9/20 and Miller Road and the other across from Town Hall. These nodes have different characters which should be reflected in their future design.

Mix of Uses: The Town Center will provide a mix of uses including retail, restaurants, residential andactivities that focus on creating a 16-hour environment that caters tothe interests and needs of seniors, children, and young adults. Targeting these demographic groupswill ensure the Town Center is active throughout the day, enhancing the Town's sense of place.

Manage US 9/20: From I-90 Exit 11 to Miller Road, the character of US 9/20 should bedifferent from the rest of the corridor in Town. Complete Street and boulevard style design techniques, combined with local enforcement, can assist with lowering travel speeds by motorists, increasing bicycle and pedestrian travel options and improving the overall look of the corridor.

Keep it Schodack: The Town Center will complement the existing rural charm of the community by creating a walkable community c enter that creates a sense of identity. The development nodes will be limited in size so they are focused, impactful and support the rest of the community.

5-Minute Walk: Most people are comfortable walking for 5 minutes, or about ¼ mile. This distance will be the benchmark for developing the walkable nodes in the Town Center.

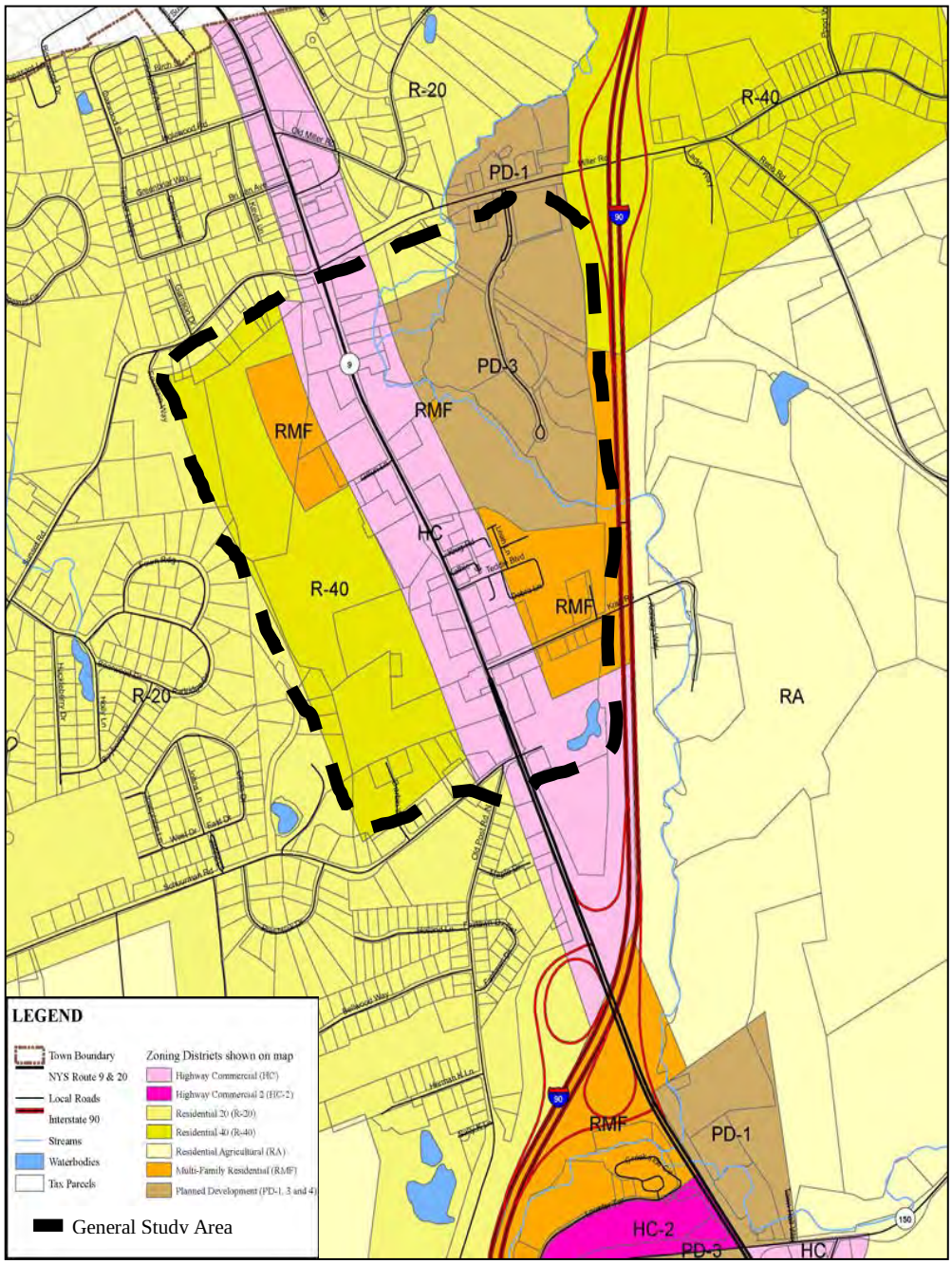
With these principles in mind, the Town Center Plan proposes to create a cohesive pedestrian environment along US 9/20 by bringing buildings close to the street, adding continuous sidewalks and creating a street grid when possible with an interconnected network of walkable streets.Implementation of this Plan will not only bring to fruition the Town's vision of a place where living, working and playing are synonymous, but it will also preserve the family-oriented community for generations to come.

## Zoning and Design Guidelines for the Town Center

The *Schodack Town Center Plan*outlines several steps and action items that the Town should pursue to implement a Town Center. The transformation of the area from an auto dominated environment to one that incorporates mixed-uses, walkable streets, compactness/density, good design, useable open space, appropriate parking, etc. will take time.

The Town's zoning regulations currently do not permit the type of development envisioned in the Town Center Plan (see Page 4 for the current zoning map in the Town Center study area). The Town also lacks design guidelines that support the Town Center concepts. CDTC and the Town believe that Form Based zoning for the Town Center with a related streets plan will most effectively allow for the type of mixed use, walkable development envisioned by the Town Center Plan.

The *Schodack Town Center Plan* is available on CDTC's website at http://bit.ly/1IO8xgl.




*Existing Zoning in the proposed Town Center study*

**Scope of Work**

Consultants will be required to complete the scope of work as outlined below. If, based on consultants' knowledge or experience, the consultant believes the required scope of work should be changed in any way; the suggested changes should be outlined in the letter of interest (as described in the Submission Instructions section of this REI). Consultants will not be required to reproduce or recreate this scope of work in their letters of interest. Only modifications to what is requested will be required and considered in consultant evaluations.

**Task 1.0: Study Initiation and Data Gathering (approx. 5% of effort)**

A) Review Previous and Ongoing Studies

Prior to meeting with the Study Advisory Committee the consultant will review the *Schodack Town Center Plan (2014)*to familiarize themselves with the Town's vision for the study area. The consultant will also be provided copies of any and all relevant materials/studies that pertain to the study area. Critical to this review is the identification of any gaps or missing details that might preclude the writing of a Form Based code for the study area. A list of any missing information should be created by the consultant including any broad questions that may need to be answered through a public forum.

B) Study Area Site Visit

Prior to meeting with the Study Advisory Committee the consultant along with staff from the Town and CDTC will walk through the study area to become familiar with its existingphysical attributes and potential constraints that must be considered in developing new zoning.

C) Study Advisory Committee (SAC) Meeting #1

SAC Meeting #1 will serve as the study kick-off meeting and will be led by the consultant team. The scope of work and study area boundaries will be confirmed, the study objectives and process will be discussed as will the roles and responsibilities of the study partners. In addition, any issues that may have been identified in revisiting the *Schodack Town Center Plan*, including additional existing conditions materials, expectations for Route 9&20 or other new information should be shared and discussed. The consultant should be prepared to describe the Form Based zoning approach and why it is believed to be the best approach to implement the *Schodack Town Center Plan* concepts. Any questions that remain to be answered before zoning code language can be written, including the potential date and time of the first public workshop, will be identified. The Town's web site will be utilized for input on the project and its draft products.

See the Study Advisory Committee section of this REI (Page 9) for additional detail on the study advisory committee roles and responsibilities.

*Deliverables:*

- *Brief memo summarizing additional information to be gathered or confirmed prior to the creation of a Form Based zoning code for the study area.*

- *SAC Meeting 1 summary.*

**Task 2.0: Stakeholder Interviews and Public Workshop 1 (approx. 20% of effort)**

A) Stakeholder Interviews

Stakeholder Interviews with key property owners in the study area will be held. Meetings with other key stakeholders may be held such as business owners (if not the property owner), civic organizations and institutions based in the Town Center as well as the Town Board (including the Supervisor), Town Planning Board, Zoning Board of Appeals, Rensselaer County and NYSDOT, CDRPC and CDTA, as needed. A summary of these interviews will be prepared by the consultant and shared with the Study Advisory Committee.

B) Public Workshop 1

The first public workshop will be held to introduce the public to the project, to get feedback on the desired look of the Town Center and street features through a visual preference survey (or other tool) and to get answers to any remaining questions regarding the Town Center Plan's vision that is needed to write the Form Based Code. The session will also be used to educate the public and Town officials regarding the purpose of zoning, what a Form Based Code is and how it is the most appropriate approach for the implementation of the Town Center Plan.Elected officials will be highly encouraged to attend.

Consultant stakeholder interviews and workshop responsibilities include:

1) Preparing maps, presentation materials and providing any additional materials necessary for the public workshop and stakeholder interviews.
2) Preparing a one-page flyer to advertise the meeting.
3) Facilitating and conducting the workshop and interviews.
4) Summarizing the input from the public workshop, including number of attendees, for the study advisory committee and for inclusion in the final report.
5) Providing the Town with the workshop materials to post to the Town's website.

Town of Schodack staff responsibilities include:

1) Creating and maintaining a contact list of key stakeholders (i.e. Town Board members, residents, business owners, property owners etc.).
2) Posting materials and advertising the meeting through the Town's website, e-mail and direct mailing of the flyer, etc.
3) Arranging meeting space.

C) Study Advisory Committee (SAC)Meeting #2

The SAC will be debriefed on the results of the stakeholder meetings and the Public Workshop undertaken in Task 2.0. The SAC will discuss how this and any other input received may impact the development of the Form Based Code and related street design guidelines.

*Deliverables:*

- *Stakeholder interview summary.*
- *Public workshop materials, handouts, and presentations.*
- *Public workshop meeting summary.*
- *SAC Meeting 2 summary.*

**Task 3.0: Draft Form Based Code Language (approx. 30% of effort)**

The consultant will create a Draft Form Based zoning code that includes street layout and design standards in collaboration with the Town and CDTC staff. Decisions regarding the number of zoning districts, street typology, design characteristics, etc. will be made in consultation with the Town and CDTC staff and will be shared with the study advisory committee for feedback. The draft zoning language should account for the incremental change more than likely needed to implement the study area vision using the Illustrative Plan and detailed concept plans in the *Schodack Town Center Plan* as a starting point.

A) Form Based Zoning Code Considerations

The Town desires that the following be prioritized in the draft Form Based Code, guided in part by the input received at stakeholder meetings and Public Workshop 1:

- *Regulating Plan*: The regulating plan will define the number and type of zones/districts in the study area and will illustrate through a map the location of streets, blocks, open spaces (such as greens, squares, and parks), and other special features. Definitions will be provided along with the principles and intent of the code and an allowed use table. The regulations will be explained in clear user-friendly language and images.
- *Building Form*: The form of buildings will be defined and illustratedthrough regulations that consider building types, build to lines, frontage types, and building function. The location of each building type will be identified in the regulating plan.
- *Site, Street and Public Space Standards*: The standards for sites, streets and a wide range of public spaces will be defined and illustrated including, but not limited to, sidewalks, street cross sections, parking lots, street trees, green spaces/landscaping, storm water, outdoor lighting, etc. Street standards will incorporate a complete street design philosophy.
- *Signage Standards*: Regulations will be developed that control the allowable signage sizes, materials, illumination, and placement.

The selected consultant may incorporate these elements into a draft code that most appropriately meets the needs of the Town of Schodack.Per federal requirements, information will be included as an appendix to the draft Form Based Code pertaining to CDTC's Americans with Disabilities Act, Environmental Justice and Environmental Mitigation requirements. CDTC staff will assist with meeting these requirements.

Note: The Form Based Code must be integrated into Schodack's existing regulatory framework (zoning and land development regulations) in a manner that insures procedural consistency,

complies with state and local legal requirements, provides clarity as to applicability of existing regulations, and maximizes the effectiveness of the code. It must also consider the update to the Town wide zoning code. Administration of the Form Based Code (such as the role of the Town Planner, Planning Board responsibilities and the development review process to be used) will be determined by the Town of Schodack.

B) Study Advisory Committee Meeting #3

The committee will review and offer comments on the draft From Based Code. The deliverables completed as part of Task 3.0 will be provided to the study advisory committee one week prior to this meeting. The date and time of the Town Board workshop presentation will be discussed.

*Deliverables:*

- *Draft Form Based Code.*
- *SAC Meeting 3 summary.*

**Task 4.0: Stakeholder Outreach and Public Input Workshop 2 (approx. 25% of effort)**

All major property owners in the study area will be consulted regarding the draft zoning code in order to gather direct input. The consultant will present the draft zoning code to the public and Town officials (especially the Town Board) to gather comments and open a 45-day review period. The presentation will be made at a Town Board Workshop or at a single purpose public meeting, and may include a joint gathering of municipal boards and committees, as determined by the Town of Schodack.

Copies of the draft form based code will need to be made available one week prior to this meeting in hardcopy and in digital form, posted on the Town's website. Study advisory committee members will be highly encouraged to attend the workshop.

Consultant stakeholder outreach and workshop responsibilities include:

1) Preparing maps, presentation materials and providing any additional materials necessary for stakeholder outreach and public input.
2) Preparing a one-page flyer to advertise the meeting.
3) Facilitating and conducting the workshop and stakeholder outreach.
4) Summarizing the input from the public workshop, including number of attendees, for the study advisory committee and for inclusion in the final report.
5) Providing the Town with relevant materials for posting to the Town's website.

Town of Schodack staff responsibilities include:

1) Creating and maintaining a contact list of key stakeholders (i.e. residents, business owners, property owners etc.).
2) Participating, along with the consultant, in the stakeholder outreach process.
3) Posting materials and advertising the meeting and draft code through the Town's website, e-mail and direct mailing of the flyer, etc.

4) Arranging meeting space.

*Deliverables:*

- *Stakeholder outreach summary.*
- *Public input workshop materials, handouts, and presentations.*
- *Public input workshop summary.*

**Task 5.0: Final Draft Zoning Code and Presentation (approx. 20% of effort)**

Following the 45-day review period, revisions to the draft code in response to comments will be incorporated into a Final Draft Zoning Code to be presented to the Town Board. A presentation will be given on the overall code changes since the draft was issued and a discussion regarding next steps for implementation will be provided. Study advisory committee members will be highly encouraged to attend.

*Deliverables:*

- *Summary of comments received during the 45-day review period and responses to those comments.*
- *Final Draft Form Based Code.*

**Steering Committee and Study Advisory Committee (SAC)**

CDTC and the Town of Schodack will jointly manage and serve as the steering committee for the project. Final decision making authority rests with the steering committee. A study advisory committee will be created with numerous representatives from the Town of Schodack and representatives from CDTC, CDRPC, NYSDOT and Rensselaer County (all as needed) to guide the study and meet with the consultant on, at minimum, three occasions as described in the scope of work.

All deliverables to be reviewed at study advisory committee meetings and public meetings must be received by committee members at least one week prior to the meeting. The consultant will be responsible for distributing deliverables to the study advisory committee via email or printed copy.

**Deliverables**

The consultant will be responsible for providing the following deliverables:

Task 1.0:

- Brief memo summarizing additional information to be gathered or confirmed prior to the creation of a Form Based zoning code for the study area.
- SAC Meeting 1 summary.

Task 2.0:

- Stakeholder interview summary.
- Public workshop materials, handouts, and presentations.
- Public workshop meeting summary.

- SAC Meeting 2 summary.

Task 3.0:

- Draft Form Based Code.
- SAC Meeting 3 summary.

Task 4.0:

- Stakeholder outreach summary.
- Public input workshop materials, handouts, and presentations.
- Public input workshop summary.

Task 5.0

- Summary of comments received during the 45-day review period and responses to those comments.
- Final Draft Form Based Code.

# TOWN OF SCHODACK
# Town Center Zoning for Route 9/20

4/18/2016

Dear Property Owner:

The Town of Schodack is in the process of developing form-based zoning codes to implement its Town Center Plan for Route 9/20. These regulations will transform the area from an auto-dominated environment to one that incorporates a mix of uses, walkable streets, compactness/density, good design, useable open space, and appropriate parking.

As a property owner in the Town of Schodack Town Center, you are being personally invited to join us in a one-to-one discussion with other key stakeholders about the proposed Town Center Form-Based Zoning Codess at Town Hall on **May 10, 2016**.

During our meeting with you, we will share information about:

- What is a form a based code?
- What is a regulating plan?
- What are the Town Center's Street types?
- How and where will it be used in Schodack?
- How will the proposed zoning affect your property?

You will also have an opportunity to share your feedback on specific code elements, such as but not limited to:

- Density
- Height
- Uses
- Streetscape
- Parking
- Building Envelope

**Meeting Details:** Participation is voluntary. The meeting is intended to foster an interactive environment and encourage you to share your ideas and concerns about the proposed Town Center Form-Based Zoning. The open house will be held on:

**May 10, 2016, 9:00 am to 4:00 pm**
Town of Schodack, Town Hall 2nd Floor
265 Schuurman Rd., Castleton, NY 12033

**Please RSVP by May 3rd, 2016** so we may ensure adequate staffing and time to speak with each property owner to answer any of your questions or feel free to stop in at your convenience anytime during the day. If you are unable to attend during the day, please join us in the evening for the **Public Meeting May 10, 2016 at 7:00 pm** (see attached flyer). We are committed to providing all property owners in the Town Center an opportunity discuss the proposed changes. You may direct your RSVP and any questions about the meeting to:

**Nadine Fuda, Planning & Zoning Director**
Town of Schodack
nadine.fuda@schodack.org
(518) 477-7938
www.schodack.org

**Nicole Allen, AICP, Planning Services Manager**
Laberge Group
nallen@labergegroup.com
(518) 458-7112

Thank you and we look forward to seeing you on May 10th!

DOVER, KOHL & PARTNERS
town planning

Laberge Group
ENGINEERING ARCHITECTURE SURVEYING PLANNING
4 Computer Drive West • Albany, New York 12205
www.labergegroup.com

Capital District Transportation Committee
One Park Place Albany NY 12205 518-458-2161

Mr. Paul Petalas
23 Patalas Drive
Ease Greenbush, NY 12061

Mr. James Giacone
1640 Columbia Tpk.
Castleton, NY 12033

Cook Family
2813 Phillips Rd.
Castleton, NY 12033

Mr. Thomas Gries
2711 Sheep Meadow Ln.
Hulett's Landing, NY 12841

Reliable Properties, LLC
755 Columbia Tpk.
East Greenbush, NY 12061

Mr. Bruce Slater
5 Sandra Ln.
Rensselaer, NY 12144

Mr. Brian Hart
2 Cooper Ave
Rensselaer, NY 12144

Mr. Eric Hart
2527 Brookview Rd.
Castleton, NY 12033

Mr. William Norkus
1531 Columbia Tpk.
Castleton, NY 12033

Mr. Richard Broughton
P.O. Box 276
East Greenbush, NY 12061

Mr. James Scott
141 Beaver Road
Nassau, NY 12123

Mr. Dennis Dowds
265 Schuurman Rd.
Castleton, NY 12033

Mr. Kurt Nusbaum
1550 Columbia Tpk.
Castleton, NY 12033

Mr. Rukhsana Iftikhar
752 Columbia Tpk.
East Greenbush, NY 12061

Mr. Kurt Nurnberg
175 Tsatsawassa Lake Rd.
East Nassau, NY 12062

R&P Realty Latham II,LLC
550 Troy Schenectady Rd.
Latham, NY 12110

Mr. Mark Nusbaum
105 Chamberlain Hill
Apt # 4
East Greenbush, NY 12144

Mr. Brian Hart
2 Cooper Ave
Rensselaer, NY 12144

Mr. William Hart
1 Huntswood Lane
East Greenbush, NY 12061

Mr. Stanley Paddock
1537 Columbia Tpk.
Castleton, NY 12033

Mr. Brian Feldman
470 Gaskin Lane.
The Villages, FL 32162

Ms. Kim Fusco
1 Herrington Ave
East Greenbush, NY 12061

SJT Realty, LLC
1629 Columbia Tpk.
Castleton, NY 12033

Mr. Stephen Bakerian
756 Columbia Tpk.
East Greenbush, NY 12061

Cornerstone Lands LLC
37 Lashway Ln.
East Greenbush, NY 12061

Mr. Norman Stahlamn
775 Columbia Tpk.
East Greenbush, NY 12061

Mr. Mark Teliska
2431 Old Coach Dr.
Castleton, NY 12033

1526 Columbia Tpk. LLC
1528 Columbia Tpk.
Castleton, NY 12033

Mr. Brian Moore
1533 Columbia Tpk.
Castleton, NY 12033

Mr. John Edwards
1542 Columbia Tpk
Castleton, NY 12033

Mr. Henry Hosley
P.O. Box 429
East Greenbush, NY 12061

Mr. Neil Gregory
1547 Columbia Tpk.
Castleton, NY 12033

Mr. Robert Gibellino
21 Ken Oaks Drive
Danbury, CT 06811

Miss. Cynthia Henninger
1555 Columbia Tpk.
Castleton, NY 12033

Mr. Frank Delardi
P.O. Box 417
East Greenbush, NY 12061

Mr. Daniel Carozza
1561 Columbia Tpk.
Castleton, NY 12033

Mr. Mark James Young
1560 Columbia Tpk.
Castleton, NY 12033

Mr. James Jeannin
1653 Columbia Tpk.
Castleton, NY 12033

PS & Sons LLC
1565 Columbia Tpk.
Castleton, NY 12033

Ms. Sharon Finney
87 Mt. View Ter.
Rensselaer, NY 12144

Mr. Philip Brown
1 Brown Drive
Castleton, NY 12033

Mr. David Crawmer
1590 Columbia Trk.
Castleton, NY 12033

Mr. Edmund Novak
1500 Sunset Rd.
Castleton, NY 12033

Mr. Joseph Scott
1601 Columbia Tpk.
Castleton, NY 12033

Stewarts Ice Cream Co.
P.O. Box 435
Saratoga Springs, NY 12866

Mr. Jeffrey Fountain
5 Brashear Pl.
Castleton, NY 12033

Mr. John Szczesniak
1611 Columbia Tpk.
Castleton, NY 12033

Mr. Andrew Slade
1614 Columbia Tpk
Castleton, NY 12033

Provincial Holding LLC
1572 Columbia Tpk.
Castleton, NY 12033

1618 Columbia Tpk.
363 Schodack Drive
Castleton, NY 12033

Ms. Ann Marie Vadeny
24-48 Rocky Rd.
West Coxsackie, NY 12192

Mr. Edward Nolan
37 Northgate Dr.
Albany, NY 12203

Ocar Agency Inc.
1435 Schodack Valley Rd.
Castleton, NY 12033

Fox Hollow LLC.
P. O. Box 357
East Greenbush, NY 12061

Mr. George Galib
1640 Columbia Tpk
Castleton, NY 12033

MK-Menlo Property Owner
c/o Rite Aid Corp
P.O. Box 3165
Harrisburg, PA 17105

Schodack Reality Assoc. LLC
16 Mount Ebo Rd. Ste 22
Brewster, NY 10509

Center Square Properties
1008 South Schodack Rd.
Castleton, NY 12033

Greenmeadow Addoc.
654 Columbia Tpk.
Castleton, NY 12033

Carl Gray
Curtis Lumber
1657 Columbia Tpk.
Castleton, NY 12033

Mr. William Norkus
1531 Columbia Tpk.
Castleton, NY 12033

SHS Holdings LLC
4 Springhurst Dr. # 207
East Greenbush, NY 12061

Mr. Frank Delardi
P.O. Box 417
East Greenbush, NY 12061

# Town Center Form Based Zoning Project

***PROPERTY OWNERS ROUNTDABLE***
**TOWN OF SCHODACK, Town Hall**
**MAY 10, 2016**
**9:00 a.m. – 4:00 p.m.**

Meeting Notes

**Major Themes/ Concerns and if appropriate Town and Consultant Response**

- Concerns that current uses will be zoned out, particularly residential.
    - Residents assured use shall be permitted to continue until any changes are made to a property or structure; procedure of such was discussed.
- Schodack Plaza owners were very excited about redevelopment opportunity and receptive to working with the Town and Consultants to achieve ultimate goals.
- Concern for the Planning Board's ability to make informed decisions on new zoning code.
    - Assured the guiding principles of board decisions shall remain unchanged, PB members will have to learn new code as in any zoning change. Annual training requirements were mentioned.
- Liked the location of Town Center being around Town Hall and Schodack Plaza. Agreement that this is a logical focal area to encourage more density.
- Discussion of allowable lot sizes and bulk requirements and concern they are in accordance and considered in the modifications to code.

**Comments and Discussion of Specific Elements**

- Questioned how parking would be determined with higher density. Questioned whether would be codified and calculations for such would be determined.
- Drive Thru should be kept to the rear of the building, screened from road views.
- Sewer capacity was questioned, whether current sewer would be adequate based on presentation images.
- Road standards and road capacity was adequate based on renderings, and if new zoning would necessarily trigger upgrades and if so when and how shall they be paid for?
- Town roads that are not to Town Standards should be maintained by developer with access easements to ensure perpetual ingress and egress into business and sites.
- Tractor Supply seen as major business and draw to this portion of town; concern new zoning will not allow for this type of business.
    - Was reiterated that infill will be encouraged in order for satisfactory integration of former regulations and Form Based Codes.
- Question of phasing of projects; if it will be required over a certain threshold of development (acres/ square footage).

Property Owner Meeting
9AM - 4pm

# TOWN OF SCHODACK
## TOWN CENTER ZONING FOR ROUTE 9/20
## TOWN HALL ** MAY 10TH, 2016

Laberge Group
ENGINEERING ARCHITECTURE
SURVEYING PLANNING
4 Computer Drive West • Albany, New York 12205
(518) 458-7112 • www.labergegroup.com

| Name (Please Print) | Affiliation / Property Address | Phone | E-mail |
|---|---|---|---|
| PHIL BROWN | 1586 COL-TPK | 477-5785 | |
| CYNTHIA HENNINGER | 1555 COL. TPKE | 477.9751 | |
| PETER NOMER | ADAMS CITY - COL TPKE | 441-3431 | |
| Richard Dumm | Dumm Vet Hosp / 1639 Columbia Tpk | | |
| JIM JEANNIN | 1563 COLUMBIA TPK | 477-9828 | |
| STAN PADDOCK | 1539 + 1537 COLUMBIA TPKE | 477-6126 | |
| Peter GAMAR | 1649 COLUMBIA TPKE | 845-2782822 | |
| Jesse Gamar | 1649 Columbia TPKE | | |
| | | | |
| | | | |
| | | | |

# PUBLIC MEETING

## TOWN OF SCHODACK
## TOWN CENTER ZONING FOR ROUTE 9/20

**When: May 10, 2016 at 7:00 PM**
**Where: Town Hall, 2nd Floor**

The Town of Schodack is in the process of developing zoning codes to implement its Town Center Plan for Route 9/20. These codes will incorporate a mix of uses, walkable streets, compactness, density, good design, useable open space, appropriate parking, etc. The meeting will open with an informational presentation on the following:

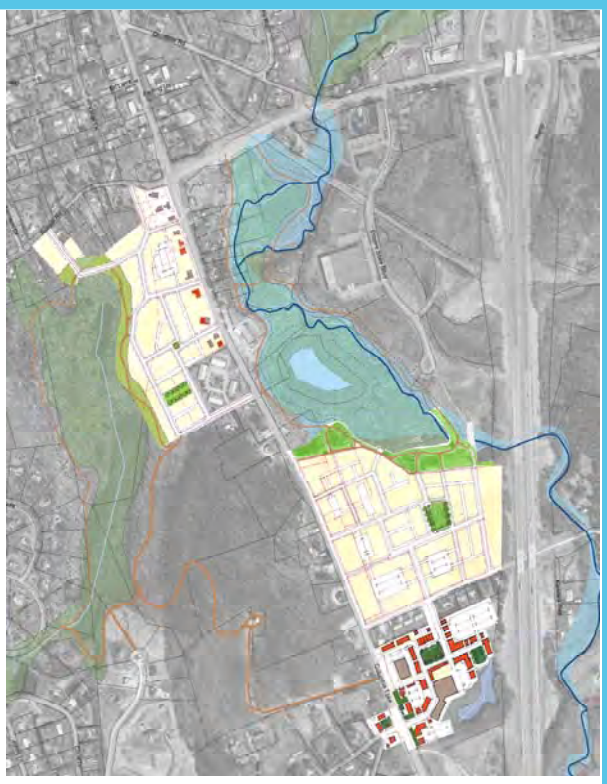
*What is a form based code?*
*What is a regulating plan?*
*What are the Town Center's Street types?*
*How and where will it be used in Schodack?*

* Following the presentation, share your ideas on various aspects of the code, including but not limited to:

***DENSITY • HEIGHT • USES***
***STREETSCAPE • PARKING***

## Join Us!!

Provide your thoughts on the
Draft Town Center Form-Based Zoning Code.



**Nadine Fuda, Planning & Zoning Director**
Town of Schodack
nadine.fuda@schodack.org
(518) 477-7938
www.schodack.org

DOVER, KOHL & PARTNERS
town planning

Laberge ENGINEERING ARCHITECTURE Group SURVEYING PLANNING
4 Computer Drive West • Albany, New York 12205
www.labergegroup.com

**Nicole Allen, AICP, Planning Services Manager**
Laberge Group
nallen@labergegroup.com
(518) 458-7112

Capital District Transportation Committee
One Park Place Albany NY 12205 518-458-2161

# Town Center Form Based Zoning Project

***PUBLIC MEETING***
**TOWN OF SCHODACK, Town Hall**
**MAY 10, 2016**
**7:00 p.m. – 9:00 p.m.**

Meeting Notes

**Major Themes/ Concerns**

- Residents all indicated they feel there has been a great deal of change in town over their lifetime, or period of time they have lived in Schodack
- Many residents were unfamiliar with Form-Based Code and what the look and feel of the community would be if development was done in accordance with these standards.
- Residents concerned that businesses now are visible from the road and any infill in front of these business would hide currently established ones.
- Concerns that single family residential uses were to be part of this district; some residents indicated displeasure as they felt this should be primarily a commercial core area.
- Residents concerned why the changes were being discussed; if it was specific to an incoming proposal, or an assumed new use.
- Concerns were discussed for areas outside the study area and if this Town Center imitative will lead to more changes in zoning.
- Some residents supported the increased density and possible vacant land infill as an increase in business in an already commercialized area of Town. Residents indicated they liked this idea due to the fact they would not be required to leave town to get some services.
- Some residents indicated they were skeptical and like the way the town has developed, they enjoyed the lack of high density intense retail and tall buildings, which they see desirable in this rural/suburban town.
- The increase in infrastructure and services needs were discussed:
    - Residents concerned who would pay for new infrastructure if needed.
    - If additional services would be offered or required to be offered on the Town level due to higher density, and how that would be paid for.

# TOWN OF SCHODACK
## TOWN CENTER ZONING FOR ROUTE 9/20
## TOWN HALL ** MAY 10TH, 2016

Laberge Group
ENGINEERING ARCHITECTURE
SURVEYING PLANNING
4 Computer Drive West • Albany, New York 12205
(518) 458-7112 • www.labergegroup.com

| Name (Please Print) | Affiliation | Phone | E-mail |
|---|---|---|---|
| JIM JEANNIN | 1563 COL TPK OWNER | 4779828 | NONE |
| Barb Spink | 2 Star Terr | 479 0713 | bcs0424@gmail.co |
| AnVadney | 1620 Col. Tpk | 2654248 | amvadney@aol.co |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

# TOWN OF SCHODACK
## TOWN CENTER ZONING FOR ROUTE 9/20
## TOWN HALL ** MAY 10TH, 2016

Laberge ENGINEERING ARCHITECTURE Group SURVEYING PLANNING
4 Computer Drive West • Albany, New York 12205
(518) 458-7112 • www.labergegroup.com

| Name (Please Print) | Affiliation | Phone | E-mail |
|---|---|---|---|
| Ray Lemka | First Cousin | 669-7184 | RLemka@NYC.rr.com |
| Cynthia Henninger | Property Owner | 477-9751 | CHLOE9657@AOL.com |
| Andrew Semeiks | Citizen | 732-7329 | asemeiks@capitalpost.com |
| Jesse Gamar | 1649 Columbia Tpke | 845 278 7822 | Jesse.Gamar@summanagementcorp.com |
| Ken Stolcon | Property owner | 732-7975 | KStolcon@esd.ny.gov |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

# TOWN OF SCHODACK
## TOWN CENTER ZONING FOR ROUTE 9/20
## TOWN HALL ** MAY 10TH, 2016

Laberge ENGINEERING ARCHITECTURE Group SURVEYING PLANNING
4 Computer Drive West • Albany, New York 12205
(518) 458-7112 • www.labergegroup.com

| Name (Please Print) | Affiliation | Phone | E-mail |
|---|---|---|---|
| Tracy Rex | Resident / Board Member | 496-4369 | trex for schodack@gmail |
| Patrick Gebbie | Resident | 269-7984 | patrick.gebbie@gmail |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

# TOWN OF SCHODACK
## TOWN CENTER ZONING FOR ROUTE 9/20
## TOWN HALL ** MAY 10TH, 2016

Laberge Group
ENGINEERING ARCHITECTURE SURVEYING PLANNING
4 Computer Drive West • Albany, New York 12205
(518) 458-7112 • www.labergegroup.com

| Name (Please Print) | Affiliation | Phone | E-mail |
|---|---|---|---|
| Denis Leonard | None | | |
| Bill Eichengrun | Sterling Oaks Comm | | |
| Stan Paddock | | | |
| Maria Paddock | | | |
| Martha Shelton | | | |
| Jim Shaughnessy | Burn Planning Board | | |
| Joe Grimaldi | | | |
| Peter Gamar | Schodack Plaza | 845 278-2822 | pgamar@sunmanagementcorp.com |
| Sharon Slade | Star Lee Boutiques, Inc | 518 225 0246 | Sharleedayspa@yahoo.com |
| Andrew M Slade | " " " " | 518-339-3106 | Aslade2@nycap.rr.com |

# TOWN OF SCHODACK
## TOWN CENTER ZONING FOR ROUTE 9/20
## TOWN HALL ** MAY 10TH, 2016

Laberge ENGINEERING ARCHITECTURE Group SURVEYING PLANNING
4 Computer Drive West • Albany, New York 12205
(518) 458-7112 • www.labergegroup.com

| Name (Please Print) | Affiliation | Phone | E-mail |
|---|---|---|---|
| Mark Moeske | Resident | 5186745673 | |
| Karyn Trossbach | Realtor | 518527-1802 | Karyn.Trossbach@gmail.com |
| CAROLINE LEFSIN | Resident | 766-0422 | |
| Richard H Dunn | Property owner | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |

Laberge Group

ENGINEERING • ARCHITECTURE • SURVEYING • PLANNING

# Town Center Form Based Zoning Project

## *COMMITTEE MEETING #2*

**TOWN OF SCHODACK, Town Hall**
**MAY 11, 2016**
**9:00 p.m. – 11:00 p.m.**

Agenda

| | | |
|---|---|---|
| **I.** | **Review**<br>• Consultant & Steering Committee Members Review of Materials and Timelines Since Meeting #1 | **10 Minutes** |
| **II.** | **Review Public Comments**<br>• Review Property Owner Meeting Comments<br>• Review Public Meeting Comments<br>• Review Additional Comments | **45 Minutes** |
| **III.** | **Review Form Based Code**<br>• Review Form Based Code Examples<br>• Discuss FBC Possibilities in Schodack<br>• Determine Desired Elements and Forms | **45 Minutes** |
| **III.** | **Next Steps**<br>• Set Committee Meeting #3<br>• Determine Draft Dates | **20 Minutes** |

4 Computer Drive West • Albany, New York 12205 • (518) 458-7112 • Fax (518) 458-1879
www.labergegroup.com

Laberge Group

ENGINEERING • ARCHITECTURE • SURVEYING • PLANNING

# Town Center Form Based Zoning Project

## *COMMITTEE MEETING #3*

**TOWN OF SCHODACK, Town Hall**
**JULY 14, 2016**
**10:00 a.m. – 12:00 p.m.**

Agenda

| | | |
|---|---|---|
| **I.** | **Review**<br>• Consultant & Steering Committee Members Review of Materials and Timelines Since Meeting #2 | **10 Minutes** |
| **II.** | **Review Draft Comments**<br>• Review Committee Comments on Draft | **55 Minutes** |
| **II.** | **Review Draft Format**<br>• Discuss Report Format and Additional Requirements | **35 Minutes** |
| **III.** | **Next Steps**<br>• Review Existing Road, Right-of-Way Dimensions.<br>• Determine Next Meeting Date | **20 Minutes** |

4 Computer Drive West • Albany, New York 12205 • (518) 458-7112 • Fax (518) 458-1879
www.labergegroup.com

Laberge Group

ENGINEERING • ARCHITECTURE • SURVEYING • PLANNING

# Town Center Form Based Zoning Project

## *COMMITTEE MEETING #4*

**TOWN OF SCHODACK, Town Hall**
**DECEMBER 15, 2016**
**10:00 a.m. – 11:30 a.m.**

Agenda

| | | |
|---|---|---|
| **I.** | **Review**<br>• Consultant & Steering Committee Members Review of Materials and Timelines Since Meeting #3 | **10 Minutes** |
| **II.** | **Review Final Draft**<br>• Review Committee Comments on Final Draft<br>• Review Public Comments on Final Draft | **45 Minutes** |
| **III.** | **Next Steps**<br>• Finalize Process for Adoption | **35 Minutes** |

4 Computer Drive West • Albany, New York 12205 • (518) 458-7112 • Fax (518) 458-1879
www.labergegroup.com

# Schodack Town Center Form-Based Code

SCHODACK, NY

*Welcome*

# Intro

# tonight's agenda

- What area is being planned?
- How was the plan created?
- What is the plan?
- What is a Form-Based Code?
- What is in the Schodack Town Center FBC?
- What are the next steps?

the team:

Town of Schodack

Nadine Fuda

Planning & Zoning Director

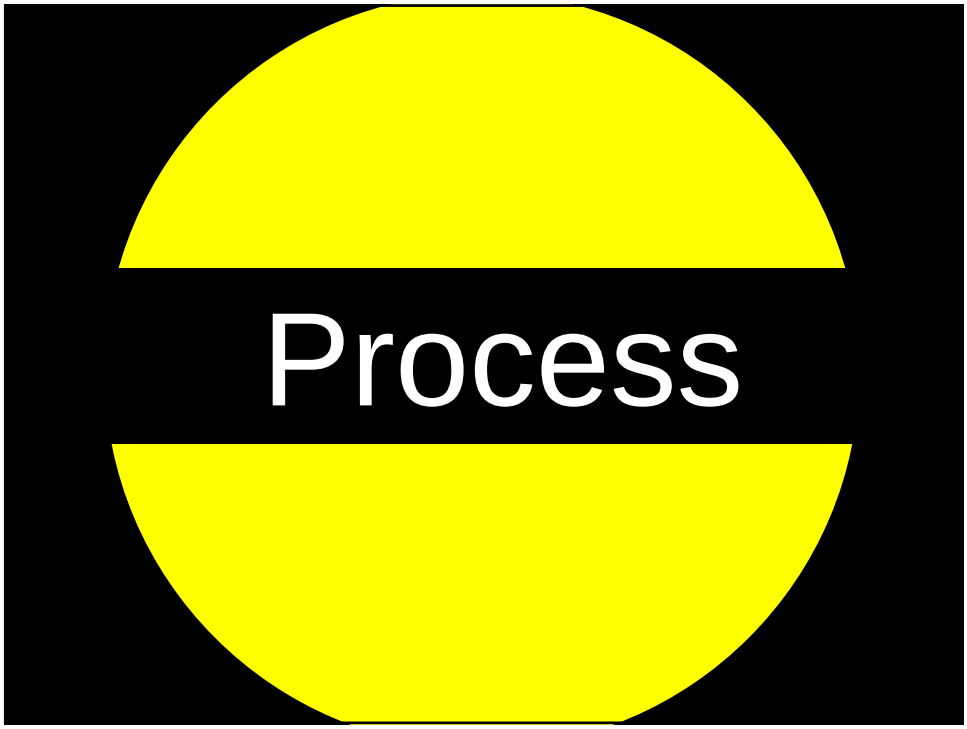
Laberge Group

planning services

Dover, Kohl & Partners

coding and design

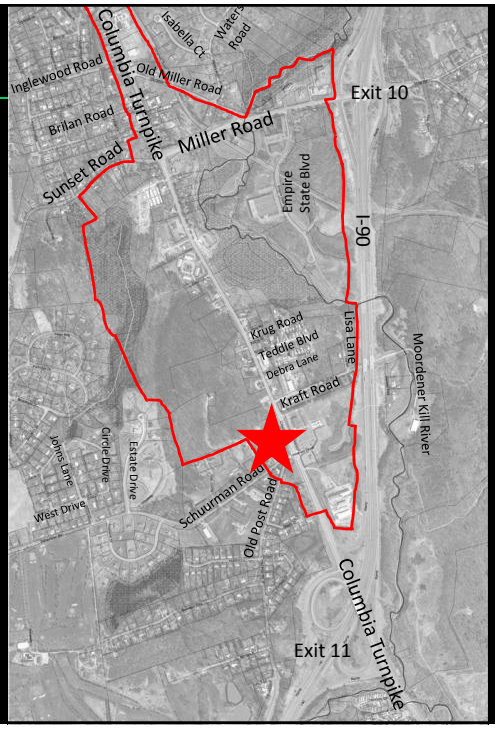
schodack.org


Process

draft

# what area is being planned?

Developing Schodack's Town Center



## Town Center

Town Hall

## Town Center

Parking Areas & Open Fields

## Town Center

Residential (Kraft Road)

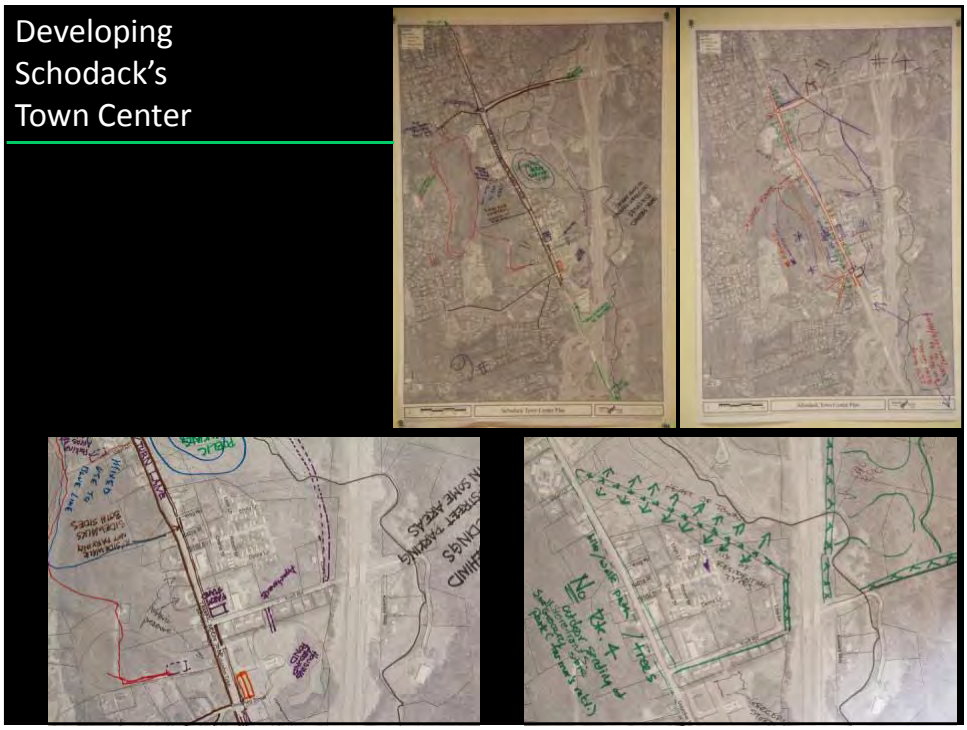
## The Plan

# how was the plan created?

Developing Schodack's Town Center

## Developing Schodack's Town Center

## Developing Schodack's Town Center



# Synthesis Map

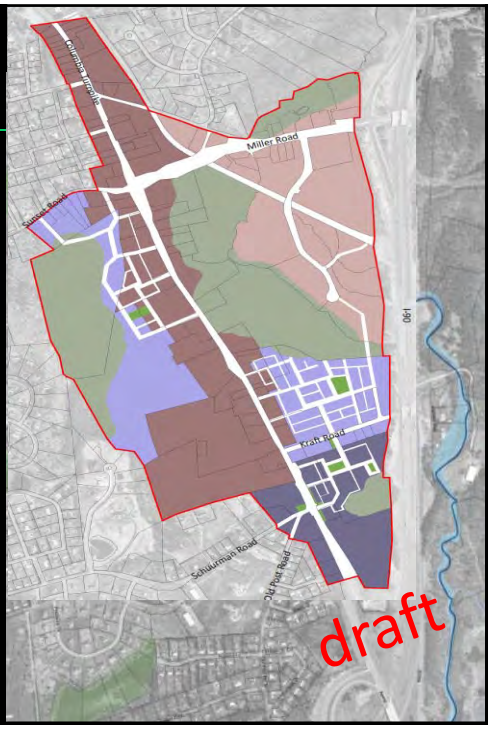
- Sense of Arrival
- Pedestrian-Oriented
- Connected Street Network
- Appropriate Scale
- Mixed Use Development
- Variety of Housing Types
- Quality Outdoor Spaces

# Illustrative Plan

- Sense of Arrival
- Pedestrian-Oriented
- Connected Street Network
- Appropriate Scale
- Mixed Use Development
- Variety of Housing Types
- Quality Outdoor Spaces

## Regulating Plan

- Sense of Arrival
- Pedestrian-Oriented
- Connected Street Network
- Appropriate Scale
- Mixed Use Development
- Variety of Housing Types
- Quality Outdoor Spaces




## Schodack's Town Center

- **Nodes of Development**
  - Different nodes of development should have different characters throughout Schodack
- **Mix of Uses**
  - Create a 16-hour environment by targeting seniors, children's & young adult interests and needs
- **Keep it Schodack**
  - Keep Schodack rural while adding uses within the nodes of development

# what is the plan?

## Schodack Town Center

existing conditions

## Schodack Town Center

step 1 – create a sense of place with the very first new buildings

## Schodack Town Center

step 1 – create a sense of place with the very first new buildings

## Schodack Town Center

step 1 – create a sense of place with the very first new buildings

## Schodack Town Center

step 2 – add buildings incrementally, making complete streets

## Schodack Town Center

step 3 – add buildings incrementally, making complete streets

## Schodack Town Center

step 4 – add buildings incrementally, making complete streets

## Schodack Town Center

step 5 – an opportunity for a larger square, framed by the fronts of buildings

## Schodack Town Center

step 5 – strengthen connections and find lost space as values increase

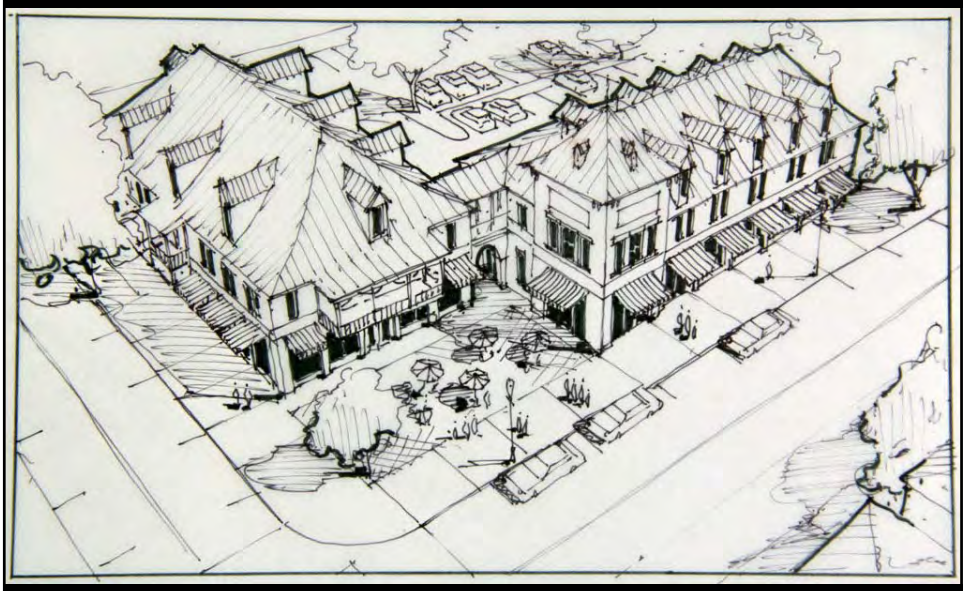
## Schodack Town Center

existing conditions

## Schodack Town Center

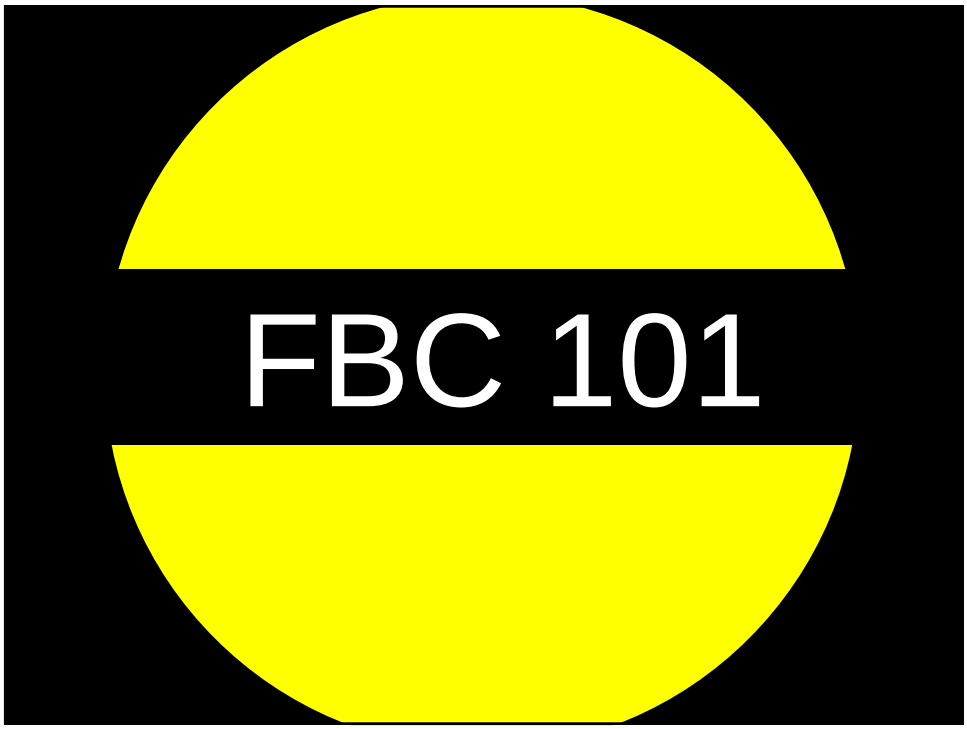
step 5 – strengthen connections and find lost space as values increase

## character



## character

# what is a form-based code?



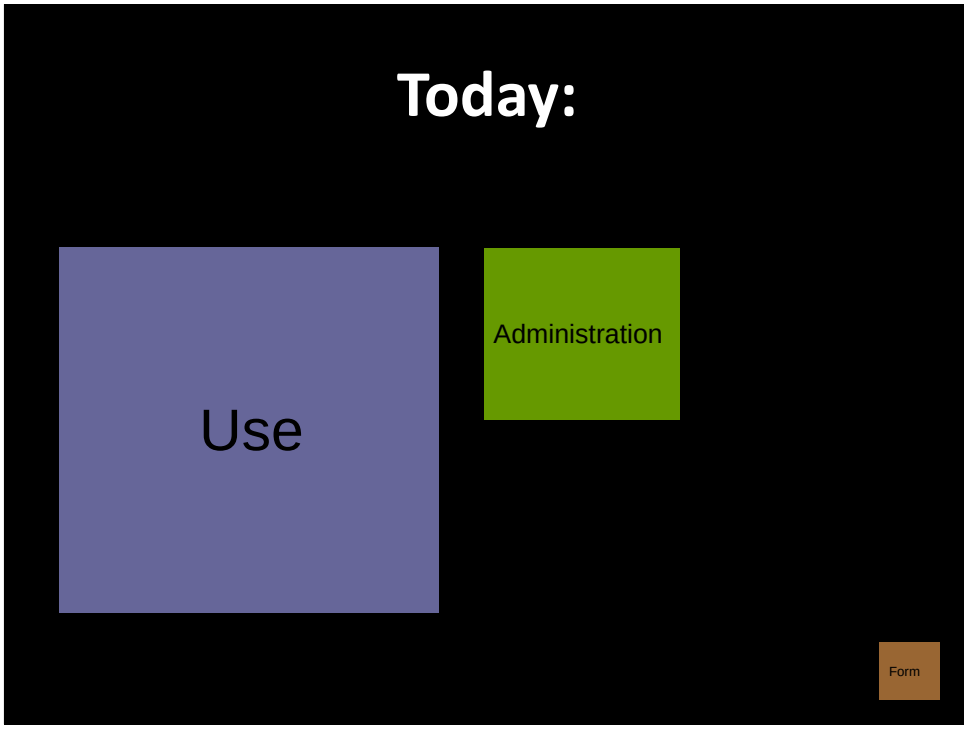
Form-based codes create a walkable public realm by regulating the form of buildings, streets and public spaces.

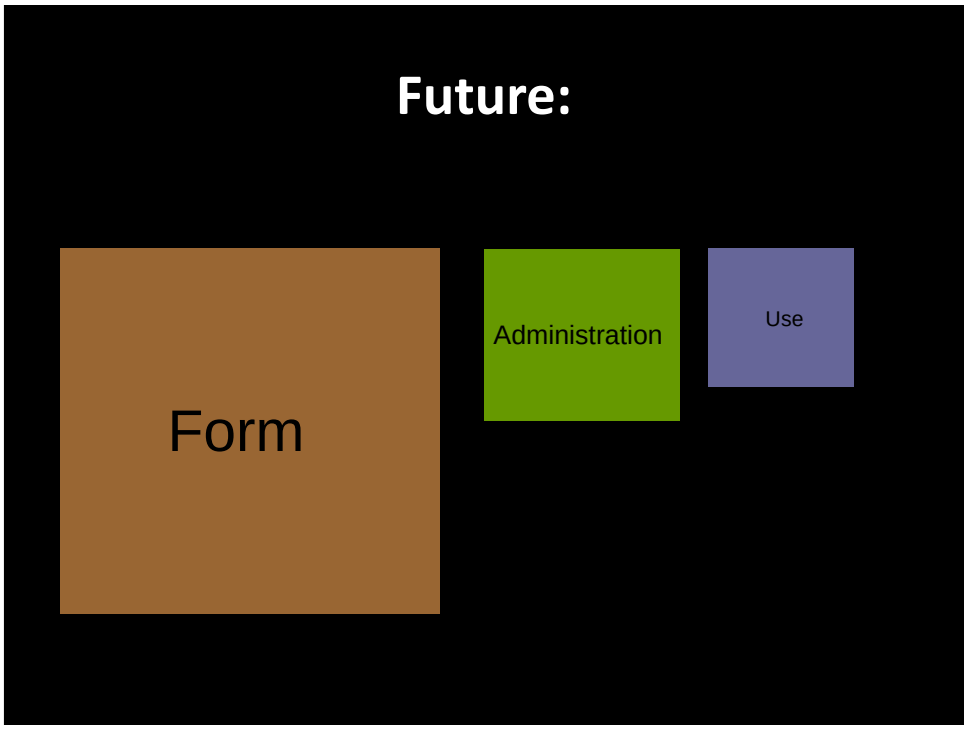
**where would you rather live?**

## where would you rather live?

## Three parts to land use regulation:

- Form
- Use
- Administration


Today:
Use
Administration
Form


Future:
Form
Administration
Use

## it's red on the zoning map

Strip Shopping Center

## it's red on the zoning map, too

Corner Store

## transforms streets

Dorn Avenue

## a new center

Serenbe, near Atlanta, GA

## a new center

New Town St. Charles, near St. Louis, MO

## a new center

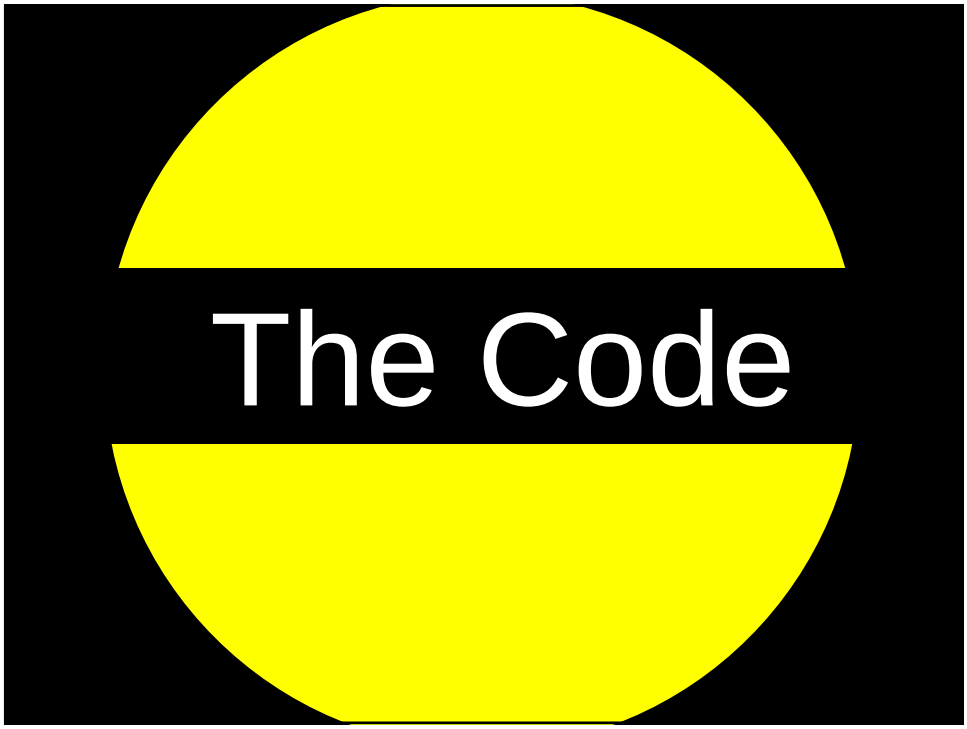
Baldwin Park, Orlando FL

## a new center

Hampstead, Montgomery AL

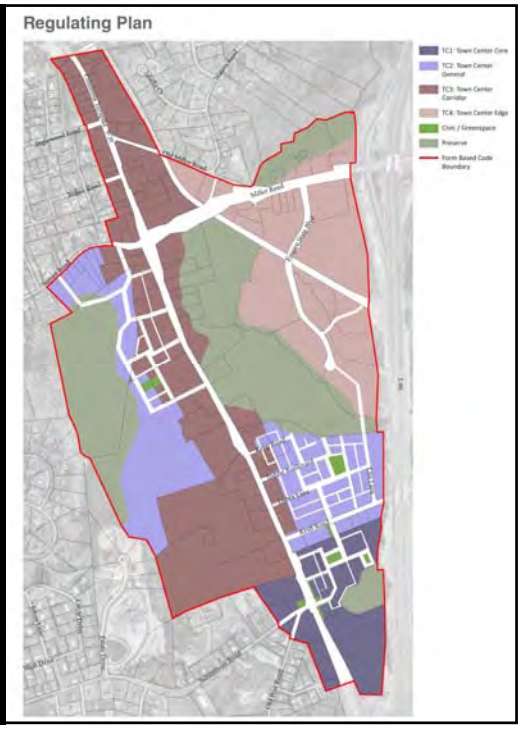
## an old center

Bristol, RI




what is in the Schodack Town Center FBC?

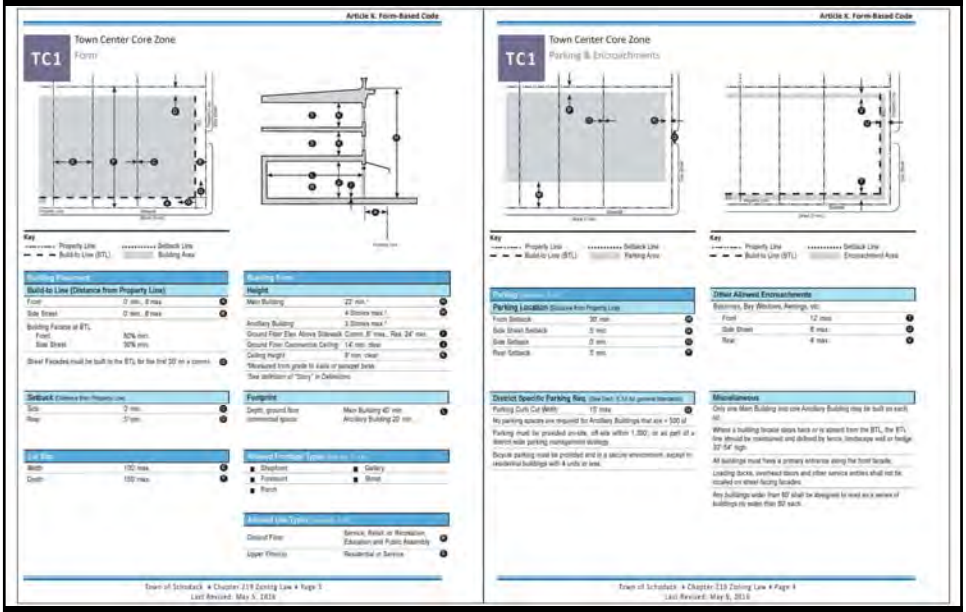
## Regulating Plan




## Form Based Regulations

TC1: Town Center Core Form Based Regulations

# Form Based Regulations




TC1: Town Center Core

# Form Based Regulations

TC2: Town Center General

# Form Based Regulations

TC3: Town Center Corridor

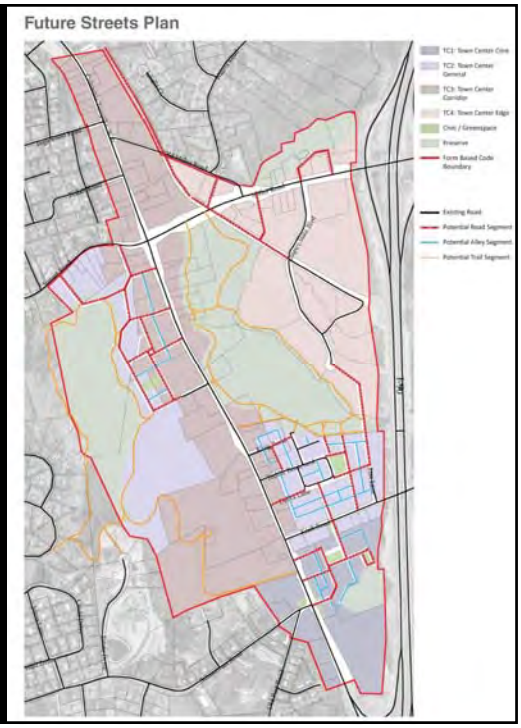
# Form Based Regulations

TC4: Town Center Edge

# Form Based Regulations

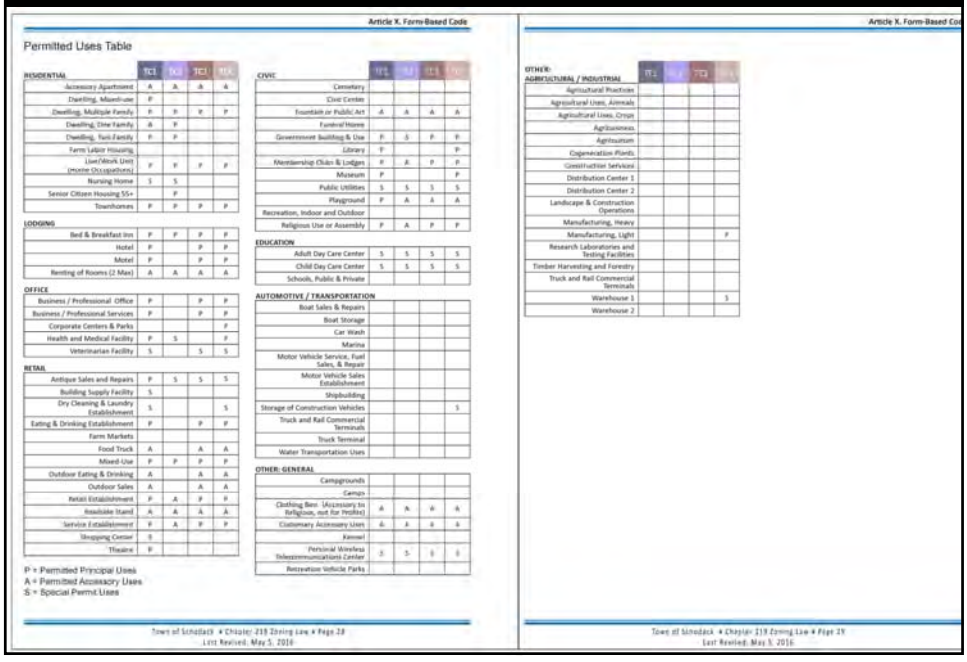
Frontage Types

# Future Streets Plan



# Street Types

# Use Table



# next steps

# commonly asked questions…

what about cold & snow?

who builds it?

what about incentives?

can this ever be built?

will the cost of development go up?

are you taking my property?

who about wetlands, steep slopes?

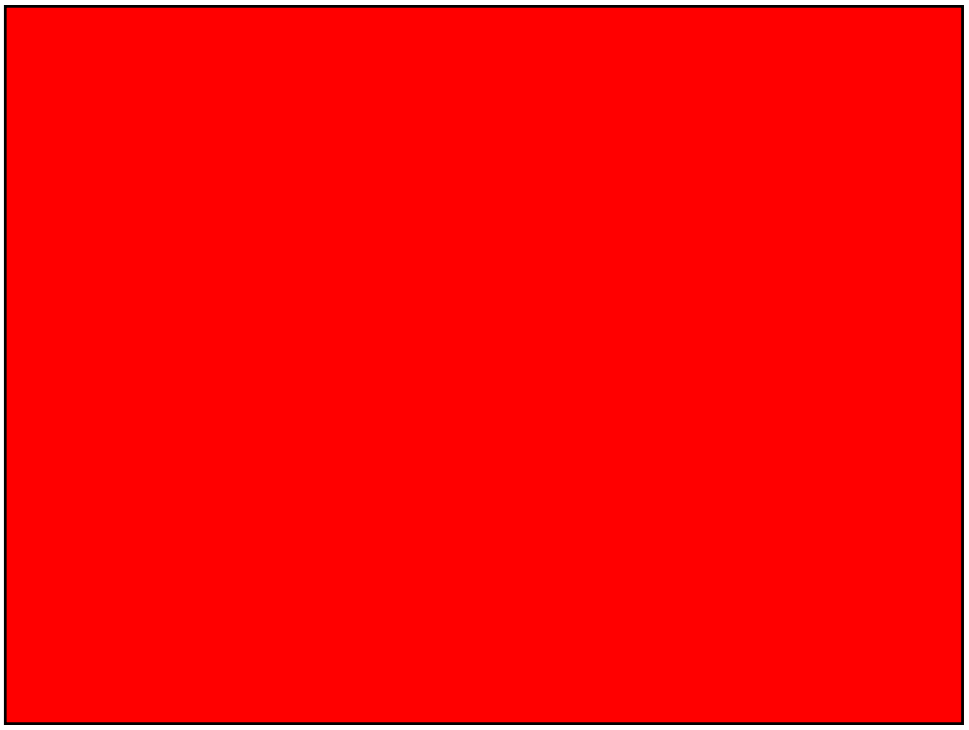
don‘t you need water & sewer?

who about the old landfill?

what are the next steps?

the conversation
continues at the boards:

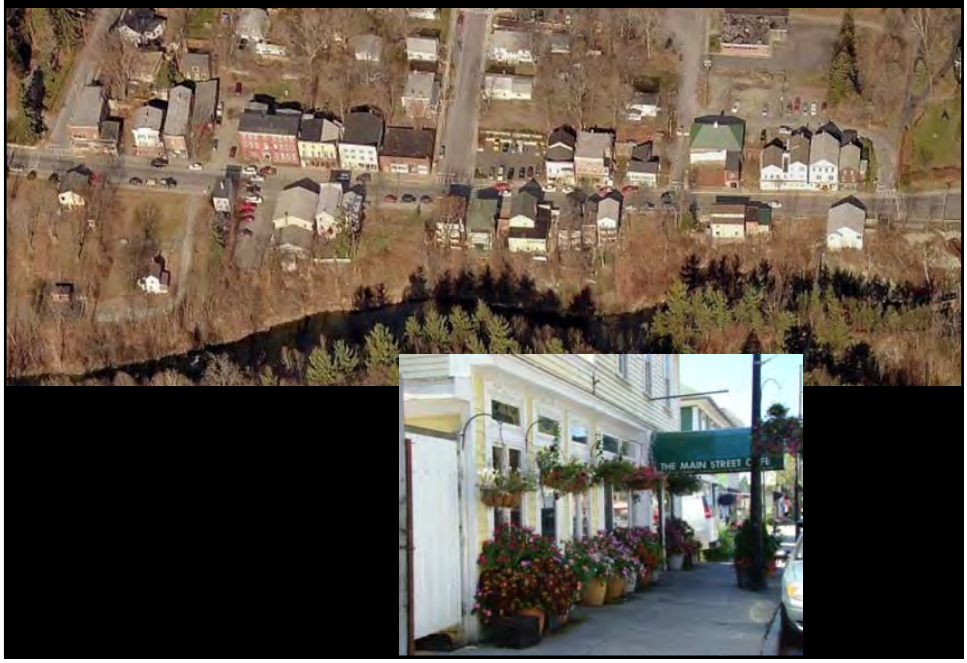
schodack.org



## Castleton-on-Hudson, NY

## Kinderhook, NY

## Valatie, NY



Variety of Housing Types

Variety of Housing Types

## diversity by design

## density without design?

# Appendix B: Environmental Justice

# Environmental Justice

## Introduction

Per federal requirements, the Capital District Transportation Committee (CDTC) undertakes an analysis of Environmental Justice in all Community and Transportation Linkage Planning Program (Linkage Program) initiatives to evaluate if transportation concepts and recommendations impact Environmental Justice populations. Impacts may be defined as those that are positive, negative and neutral as described in CDTC's Environmental Justice Analysis document, published March 2014 (available at www.cdtcmpo.org/images/plans_programs/Nondiscrimination/Environmental_Justice_2014.pdf. The goal of this analysis is to ensure that both the positive and negative impacts of transportation planning conducted by CDTC and its member agencies are fairly distributed and that defined Environmental Justice populations do not bear disproportionately high and adverse effects.

This goal has been set to:

- Ensure CDTC's compliance with Title VI of the Civil Rights Act of 1964, which states that "no person in the United States shall, on the basis of race, color, or national origin, be excluded from participation in, be denied the benefits of, or be subjected to discrimination under any program or activity receiving Federal financial assistance,"

- Assist the United State Department of Transportation's agencies in complying with Executive Order 12898 stating, "Each Federal agency shall make achieving environmental justice part of its mission by identifying and addressing, as appropriate, disproportionately high and adverse human health or environmental effects of its programs, policies, and activities on minority populations and low-income populations."

- Address FTA C 4702.1B TITLE VI REQUIREMENTS AND GUIDELINES FOR FEDERAL TRANSIT ADMINISTRATION (FTA) RECIPIENTS, which includes requirements for MPOs that are some form of a recipient of FTA, which CDTC is not.

## Data and Analysis

In developing a methodology for analysis, CDTC staff created demographic parameters using Summary File 1 data from the 2010 United States Census as well as data from the 2007-2011 American Community Survey (ACS). Threshold values were assigned at the census tract level to identify geographic areas with significant populations of minority or low-income persons. Tracts with higher than the regional average percentage of low-income or minority residents are included on Map 1 as Environmental Justice populations. Minority residents are defined as those who identify themselves as anything but white only, not Hispanic or Latino. Low-income residents are defined as those whose household income falls below the poverty line.

The transportation patterns of low-income and minority populations in CDTC's planning area are depicted in Table 1, using the commute to work as a proxy for all travel. The greatest absolute difference between the defined minority and non-minority population is in the Drive Alone and Transit categories: The non-minority population is 17.5% more likely to drive alone, slightly more likely to work

**Map 1**

Environmental Justice Populations within the Schodack Town Center
Form Based Zoning Code Linkage Project Study Area
N
Environmental Justice Population
Linkage Project Study Area
March 2017
CDTC
0
0.45
1.35
Miles

at home, 10.1% less likely to take transit, and is also less likely to carpool, walk, or use some other method to commute. The greatest absolute difference between the defined low-income population and the non-low-income population follows the same trend, with the non-low-income population 20.9% more likely to drive alone and 11.7% less likely to commute via transit.

| **Table 1. Commute Mode 4-County NY Capital Region** | | | | | | |
|---|---|---|---|---|---|---|
| By Race | Drive Alone | Carpool | Transit | Other | Walk | Work at Home |
| All Workers (16+) | 80.0% | 8.3% | 3.2% | 1.2% | 3.6% | 3.7% |
| White Alone Not Hispanic or Latino | 82.5% | 7.8% | 1.8% | 1.0% | 2.9% | 3.9% |
| **Minority** | **65.0%** | **11.0%** | **11.9%** | **2.1%** | **7.4%** | **2.6%** |
| | | | | | | |
| By Income | Drive Alone | Carpool | Transit | Other | Walk | Work at Home |
| All Workers (16+) for whom poverty status is determined | 80.7% | 8.3% | 3.2% | 1.2% | 3.0% | 3.6% |
| At/Above 100% Poverty Level | 81.7% | 8.2% | 2.6% | 1.1% | 2.8% | 3.6% |
| **Below 100% Poverty Level** | **60.8%** | **10.2%** | **14.3%** | **3.1%** | **7.7%** | **3.9%** |

Data: American Community Survey 2011 5-year estimates, tables B08105H + B08122. Other incl. taxi, motorcycle, bicycle.

The study area for the Schodack Town Center Zoning project does not include any Environmental Justice areas based on the Census Tracts present. That means the study area does not have a higher than regional percentage of minority or low income residents. That said, throughout the study process the general public was given multiple opportunities to be involved including:

- The Internet was used to display and advertise information about the study.
- Social media was used to provide information and input opportunities.
- Two formal public participation opportunities were provided, with meetings held in the evening in transit accessible locations to neighborhood residents.
- Public comment was accepted throughout the study process.
- Final products will be posted to CDTC's website, the Town of Schodack's website and on social media.

**Conclusion**

CDTC defines plans and projects with a primary or significant focus on transit, bicycling, walking, or carpool as being "positive". As the primary purpose of the Schodack Town Center Zoning project is to update the Towns' Zoning regulations to allow for higher density, mixed used development patterns that are built at a scale to support walking, biking and transit use, the Zoning project will have a positive impact on the community, including any low income or minority residents. The project makes recommendations for land use, urban design, access management, streetscape improvements, pedestrian-bicycle facilities and transit improvements which, if implemented, will provide positive benefits for all population groups in the study area.

**Environmental Features Scan**

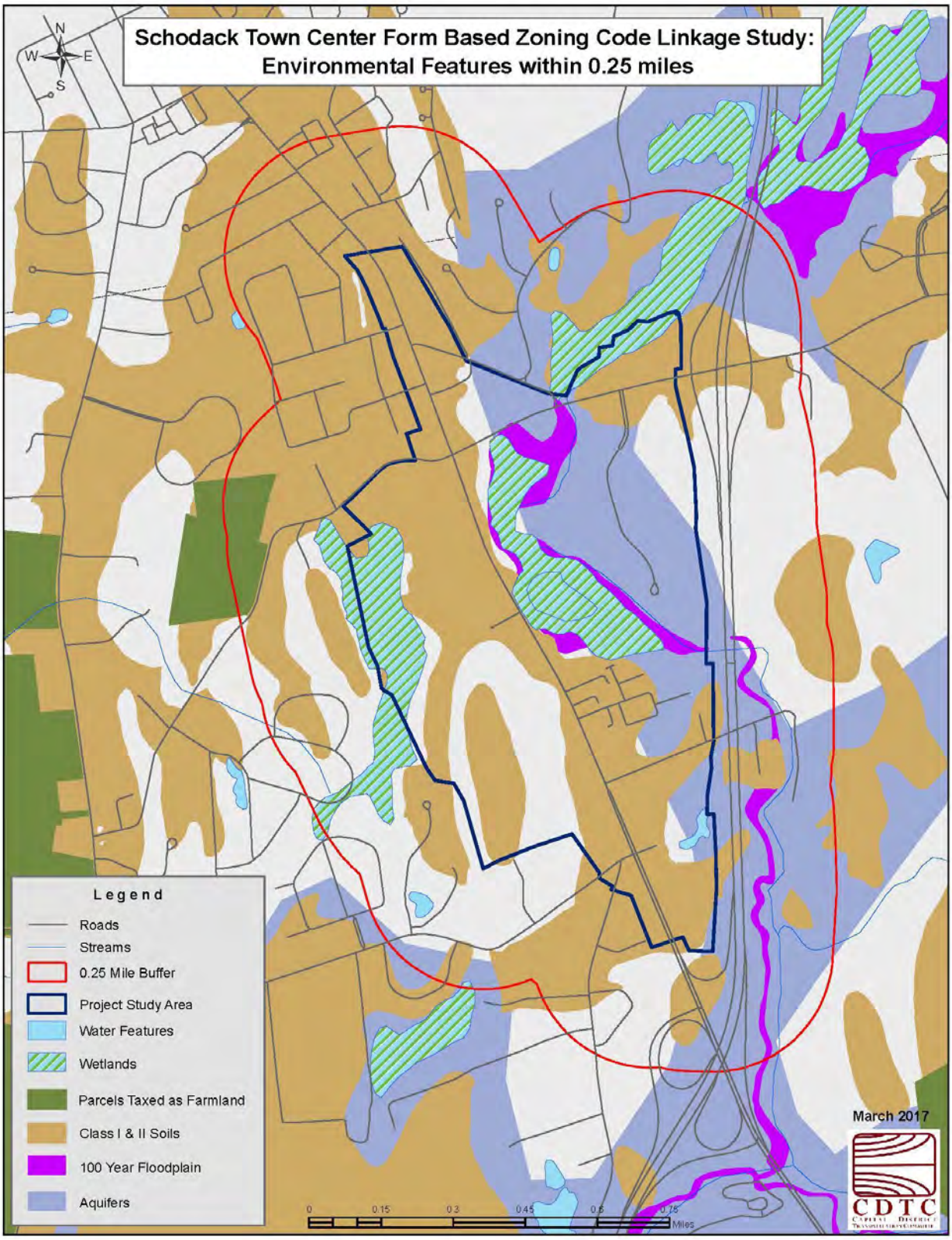
CDTC's New Visions 2040 regional transportation plan encourages smart growth as well as investment and development in urban areas as a method to protect natural resources. Smart growth policies also help to protect rural character and open space, and protect quality of life in the Capital Region. CDTC has undertaken review of natural and cultural resource mapping, and for the development of the Regional Transportation Plan consulted with federal, state and local agencies on environmental issues as an important part of the environmental mitigation process. Along with evaluating the impacts to environmental systems of candidate transportation projects for federal funds, CDTC documents the environmental systems present in the study areas for Linkage Program planning initiatives.

Map 2 provides an overview of the environmental systems present in the Schodack Town Center Zoning study area. CDTC uses GIS mapping of the below environmental systems to screen for potential project impacts. Features within 0.25 miles of the study area are included in Map 2. The Schodack Town Center Zoning recommendations are not expected to impact any identified features since the study area is largely already developed or protected, such as the wetland areas on the east side of US Route 9 & 20 which is identified as preserve in the new zoning.

Environmental features include:

- sole source aquifers
- aquifers
- reservoirs
- water features (streams, lakes, rivers)
- wetlands
- watersheds
- 100 year flood plains
- rare animal populations
- rare plant populations
- significant ecological sites
- significant ecological communities
- state historic sites
- national historic sites
- national historic register districts
- federal parks and lands
- state parks and forests
- state unique areas
- state wildlife management areas
- county forests and preserves
- municipal parks and lands
- land trust sites
- NYS DEC lands
- Adirondack Park
- agricultural districts
- agriculture parcels taxed as farmland
- agriculture parcels in farm use
- Class I & II soils

**MAP 2**


Schodack Town Center Form Based Zoning Code Linkage Study:
Environmental Features within 0.25 miles
N
W
E
S
Legend
Roads
Streams
0.25 Mile Buffer
Project Study Area
Water Features
Wetlands
Parcels Taxed as Farmland
Class I & II Soils
100 Year Floodplain
Aquifers
0
0.15
0.3
0.45
0.6
0.75
Miles
March 2017
CDTC